

STAFF REPORT
PLANNING COMMISSION

File No.: PDC09-016

Submitted: 04/24/09

PROJECT DESCRIPTION:

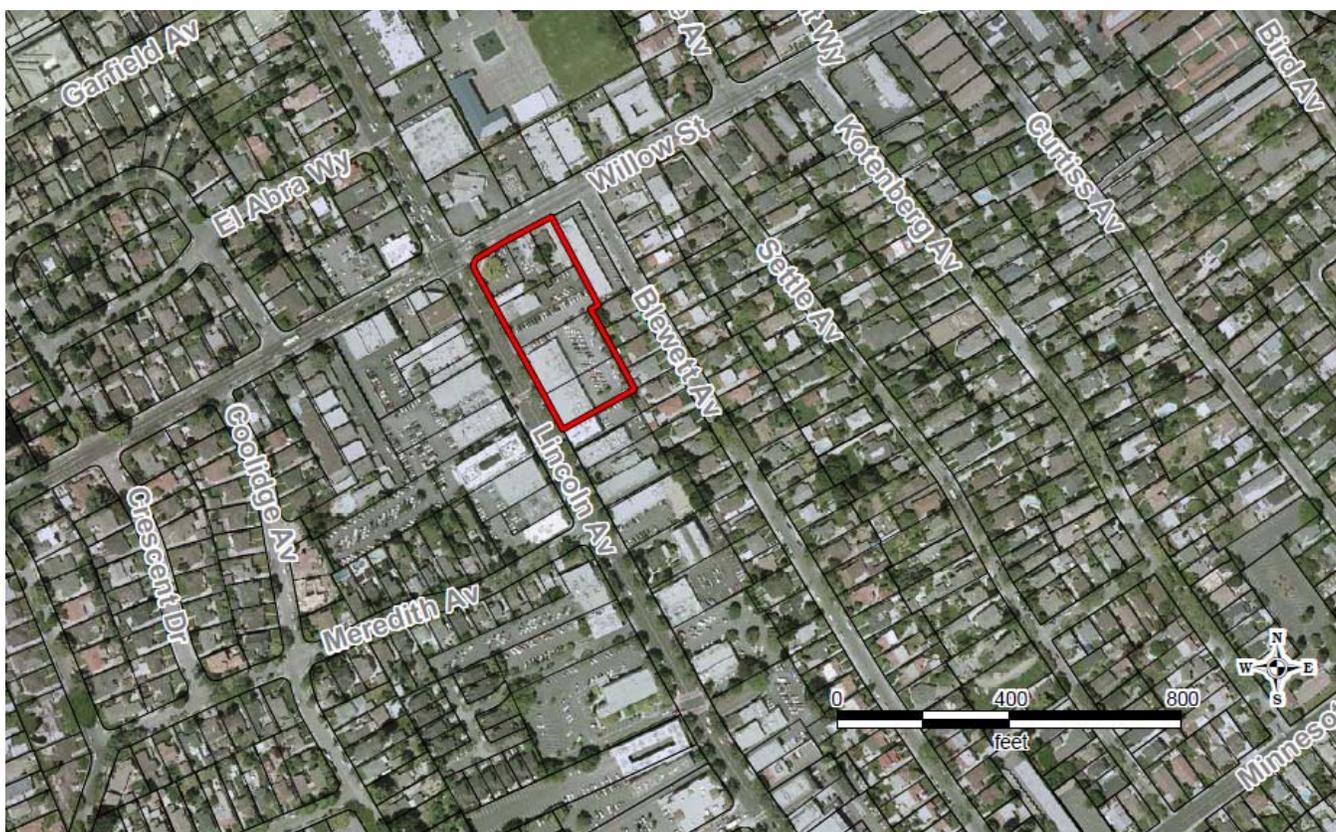
Planned Development Rezoning from the CP Commercial Zoning District to the CP(PD) Commercial Zoning District to allow 58,401 square feet of office and retail uses with a two level parking structure on a 2.08 gross acre site.

LOCATION: Southeast corner of Lincoln Avenue and Willow Street

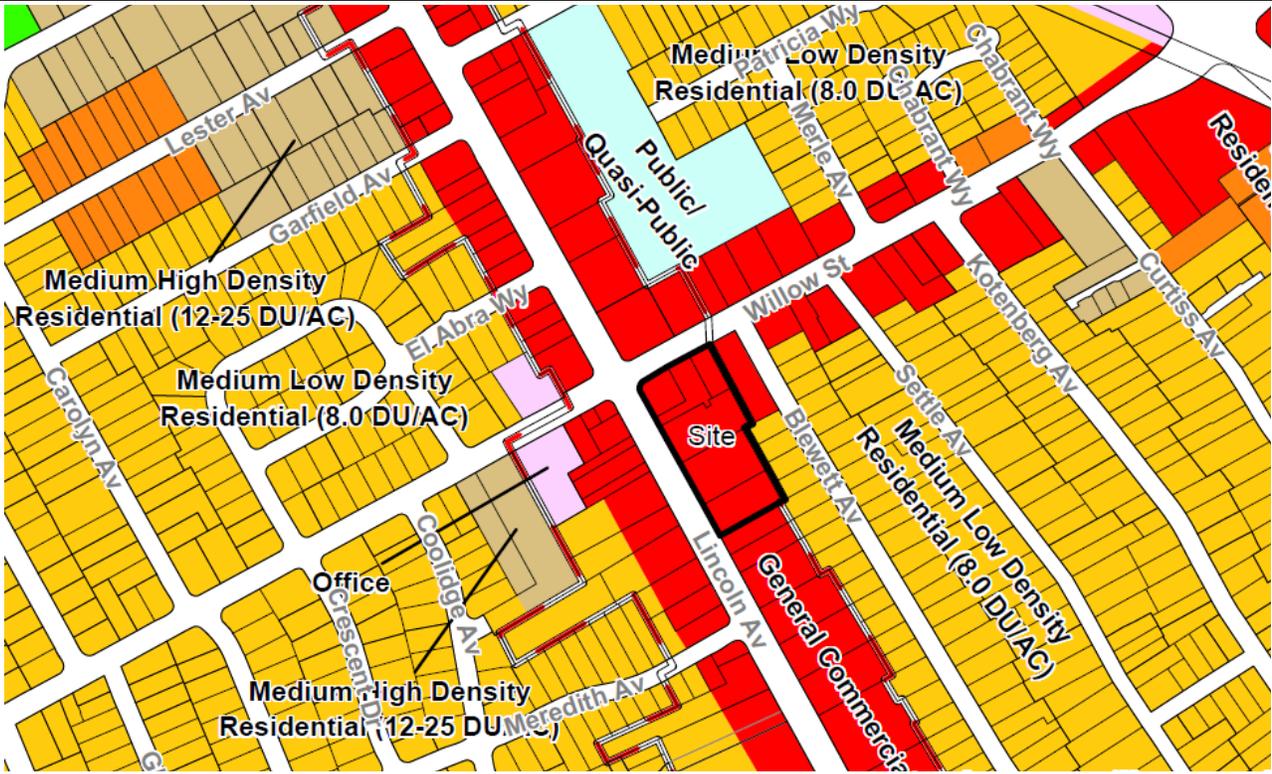
Existing Zoning	CP Commercial Pedestrian
Proposed Zoning	CP(PD) Planned Development
General Plan	General Commercial, Neighborhood Business District
Council District	6
Annexation Date	Willow Glen 10/01/1936
Neighborhood Business District	Lincoln Avenue
Historic Resource	Structure of Merit (1172 Willow Street)
Redevelopment Area	N/A
Specific Plan	N/A

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N

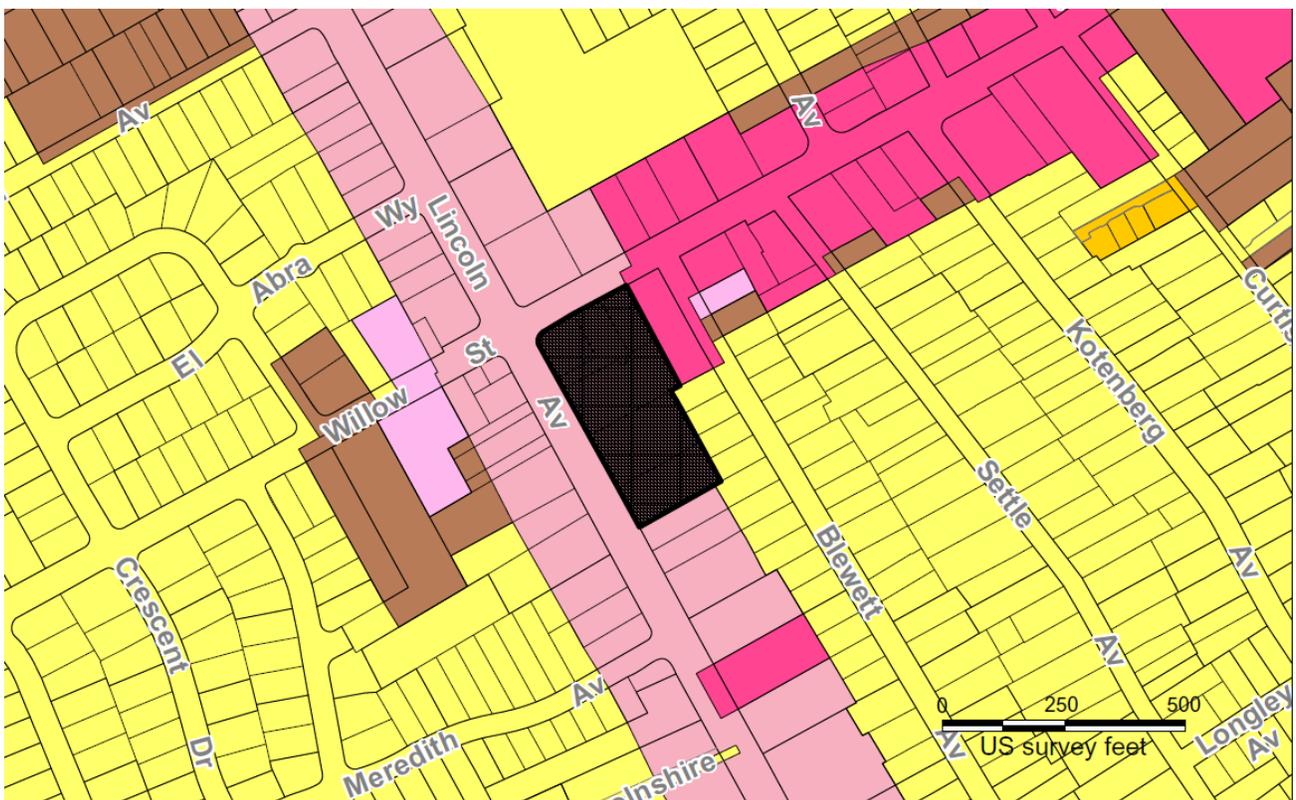
Aerial Map



GENERAL PLAN



ZONING



RECOMMENDATION

Planning staff recommends approval of the proposed Planned Development Rezoning for the following reasons:

1. The project conforms to the General Plan Land Use/Transportation Diagram designation of General Commercial and Neighborhood Business District. Further, the project conforms to key strategies and policies of the General Plan including the Economic Development Strategy, Urban Conservation/Preservation Strategy, and Urban Design policies.
2. The proposed project, as conditioned, substantially conforms to applicable policies of the City of San Jose Commercial Design Guidelines
3. The proposed project, as conditioned, is compatible with the surrounding land uses.
4. The proposed project conforms to the requirements of CEQA.

BACKGROUND & DESCRIPTION

On the April 24, 2009, the applicant, Alex Byer on behalf of Paja Investments, requested a Planned Development Rezoning to allow up to 58,401 square feet for commercial uses with an attached two level parking structure on a 2.08 gross acre site. The uses immediately adjacent to this property include commercial uses fronting on Lincoln Avenue to the north, south and west; with commercial uses fronting on Willow Street and single-family residences fronting on Blewett Avenue to the east.

A community meeting was held for the project at the Willow Glen Senior Center on September 24, 2009. Approximately 30 members of the community were present. Some community members expressed concerns in regards to the project, including; traffic issues on Lincoln Avenue, Willow Street and Blewett Avenue; access; sidewalk widths; visibility of the parking and office structures from the residential property lines; and height – particularly of the tower element. See analysis section of this report for an explanation on how these issues were addressed.

Project Description

The proposed Planned Development Rezoning of the site will consolidate three parcels into one commercial development. The focus of the Planned Development Rezoning is to construct approximately 17,258 square feet of ground floor retail with 26,120 square feet of second and third floor office above fronting on Willow Street and Lincoln Avenue. A plaza area will be created at the corner. The building heights will vary from 24 to 50 feet, except for a tower element which extends to 54 feet and will overlook the plaza. The current proposal would result in the demolition of four of the six existing buildings on the site and will incorporate the Willow Way Creamery building (1072 Willow Street), currently occupied by Willow Street Pizza, a commercial building that has been identified as a Structure of Merit. The project will also preserve an existing multi-tenant retail building on Lincoln Avenue at the south end of the site that was built within the past decade. Parking is proposed to be provided in an attached two-level parking structure and surface parking lot located at the rear of the site.

ANALYSIS

The analysis section of this report addresses the following key issues: 1) conformance to the San Jose 2020 General Plan, 3) conformance to the Commercial Design Guidelines, 4) compatibility with the surrounding land uses, and 5) conformance with the California Environmental Quality Act (CEQA).

General Plan Conformance

The subject site has a General Plan land use diagram designation of General Commercial and Neighborhood Business District Overlay. The General Commercial designation is a non-specialized commercial designation intended to permit a wide variety of commercial uses. Business and professional office uses are allowed within this category as well. As a commercial development with office uses above neighborhood serving street front retail, the proposed rezoning is consistent with this designation.

The Neighborhood Business District (NBD) overlay designation applies to commercial areas along both sides of Lincoln Avenue, which functions as a central business district, providing community focus and identity through the delivery of neighborhood-serving goods and services (e.g., banks, grocery stores, drug stores, bakeries, etc.). The NBD overlay designation is typically applied to areas with a "Main Street" design, such as Lincoln Avenue, where buildings are connected to each other to form a continuous street façade with no or minimal front setbacks. This type of building relationship creates a pedestrian oriented environment. In these "Main Street" areas, off-street parking should be located so as to minimize vehicle/pedestrian conflicts and to permit a continuous street frontage of storefronts. The proposed project will strengthen the existing Neighborhood Business District by adding commercial uses within and above existing and new retail and restaurant uses behind "Main Street" storefronts, adding a plaza for community focus, and locating parking behind the building in order to minimize vehicular and pedestrian conflicts.

In addition to conformance with the Land Use Diagram and Neighborhood Business District Overlay, the proposal is consistent with applicable General Plan strategies and other policies as noted below.

Economic Development Strategy

The purpose of the economic development strategy is to maximize the City's ability to provide adequate urban services to residents by seeking to obtain and maintain an improved balance between jobs and workers residing in San Jose. In order to maximize this ability, the City should support and encourage expansion of existing commercial areas such as the neighborhood business districts.

The proposed project would be a catalyst for new private investment in the Lincoln Avenue Neighborhood Business District. This investment would facilitate the reuse of this key pedestrian oriented shopping and business site within the Willow Glen neighborhood. Development on this site would provide additional office worker customers in close proximity to existing transportation routes and residences; thereby maintaining and/or strengthening the economic viability of neighborhood businesses while encouraging bicycle, pedestrian, and transit uses.

Urban Conservation / Preservation Strategy

The purpose of the urban conservation / preservation strategy is to conserve physical assets within the City in order to maintain areas with community identity that promote civic pride and participation. Preservation of specific structures or areas is a part of the urban conservation strategy. Historic and architectural structures add inestimable character and interest to the City's image.

The rezoning proposes to conserve and incorporate the original one-story 1935 Spanish Colonial Revival style Willow Way Creamery building (1072 Willow Street), listed on the Historic Resources Inventory as a Structure of Merit. The building is currently occupied by Willow Street Pizza, a use that is anticipated to remain. While second story office space will span over the roof of the existing structure in order to accommodate the commercial program for this project, the span will be set back behind the historic skylight, 36 feet from the front façade. This setback, together with the 11 foot adjacent ground floor setback and the alignment of the patio structure roof ridge with that of the historic building will work to respect the scale and character of the Structure of Merit within the new development.

Urban Design

The urban design goal is to require the highest standards of architectural design in order to incorporate aesthetic considerations in the development review process. While these policies recommend a maximum building height of 50 feet, they also state that height limits not exceeding 100 feet may be established in the context of project review where structures are located to avoid significant adverse effects on adjacent properties and are not designed to accommodate additional stories. A combination of various building heights are proposed, with a maximum building height of 50 feet for the three story elements primarily oriented towards Lincoln Avenue and away from the residential neighborhood to the east. This provides a reasonable separation from the nearby residential uses which was raised as a concern at the community meeting. In addition, the parking garage was limited to one above ground deck and placed to minimize the visual impacts to the adjacent residences.

The proposed development standards allow for a height of 55 feet for only the sloped roof of the tower element located behind the plaza at the corner of Willow Street and Lincoln Avenue; over 250 feet from the nearest residence to the east of the site. This slight increase in height for an integrated architectural element that breaks up the project's massing, marks the corner, and serves as an entryway feature to Lincoln Avenue, have been established in the context of the project design.

Commercial Design Guidelines Conformance and Compatibility with Adjacent Uses

Site Design/Setbacks

Primary access to the parking areas are provided from Lincoln Avenue with secondary access from Willow Street. The Lincoln Avenue driveway access includes a loading zone for the project and a 12-foot pedestrian walkway to serve the existing 15,000 square foot commercial building on the south part of the site.

The project has been carefully designed to address and respect the four key interfaces of the subject site. On the north and west, the prominent corner, pedestrian-oriented building fronts onto Willow Street and Lincoln Avenue. The corner itself is strengthened with a tower element and a roughly fifty foot square public plaza. A continuous ribbon of storefronts form a strong retail backdrop for the pedestrian realm, while second and third story office spaces are setback beyond the ground floor stores, taking advantage of rooftop terraces and views of the avenue. In accordance with concerns raised by the neighborhood, wide sidewalks are provided along both streets to encourage pedestrian activity and provide opportunities for outdoor seating. A new bus duckout is also being accommodated on Willow Street which will better improve through vehicular circulation.

Commercial uses border the project site to the east and south at the interior side property lines, where no setback is required. To be sensitive to the existing adjacent single-family residences to the east at the rear property line, the draft development standards include a minimum setback of 10 feet in order to accommodate landscaping between the parking ramp wall and the residences. The proposed setback for the three story commercial building will be setback over 100 feet from the adjacent residential uses, providing ample transition to the lower scale residential neighborhood while maximizing parking availability for the neighborhood business district.

The details of the proposed architecture will be finalized at the Planned Development Permit phase of the project. With its red clay tiled gabled roof forms, and warm stucco siding, the contemporary design takes its cues from the many Spanish Colonial Revival buildings on Willow Street and Lincoln Avenue, including the Willow Way Creamery building specifically incorporated into the new development. The

project massing consists of a substantial three story building on Lincoln Avenue, set behind a concave plaza with a slightly taller round tower marking the prominent corner. The three story building is linked to a two story building on Willow Street via the Willow Way Creamery building.

Parking

The proposed parking standards for ground floor retail and restaurant uses of one space per 400 net square feet of tenant space (1:400) is consistent with the current Zoning Ordinance requirement for neighborhood business districts. This recently adopted parking requirement is lower than the standard used for general retail (1:200) because in neighborhood business districts, trips are often combined with one customer visiting multiple retail uses and/or high levels of overall use by pedestrians. The draft development standards also include parking requirements for the office uses (on upper floors) that conform to the Zoning Ordinance requirement of one space per 250 net square feet of tenant space (1:250). Parking is being provided in a parking structure that is located to the rear of the site. This is an advantageous design as the parking structure (generally one of the least aesthetically pleasing parts of any development) will not be visible at all from the public right-of-way. In addition, the parking structure will be separated from the existing low scale residential uses with a landscaped setback and coordinated with the adjacent commercial building parking demand; consistent with the Commercial Design Guidelines.

Environmental Review

A Mitigated Negative Declaration (MND) was circulated on December 21, 2009, and is available for viewing at www.sanjoseca.gov/planning/eir/mnd.asp. The MND states that any environmental impacts that the project may have can be mitigated through various methods that will be incorporated into any permitting that is done for this property.

The buildings proposed for demolition, as well as the project site, do not qualify for listing on the National Register of Historic Places (NRHP) and/or California Register of Historical Resources (CRHR), or on the Historic Resources Inventory as Candidate City Landmarks or Contributing Structures to a City Landmark Historic District. The existing Willow Way Creamery building was evaluated by cultural resource consultants Archaeological Resource Management in a Department of Parks and Recreation (DPR) evaluation form dated October 19, 2009. The evaluation concurred with the building's current listing on the City's Historic Resources Inventory as a Structure of Merit. While, in the context of historic resources, demolition of this building would not have a significant effect on the environment as defined by CEQA, the project proposes to retain and incorporate the structure into the ground floor retail component of the new development.

A traffic study for the project was prepared by Hexagon Transportation Consultants in March 2009. Traffic issues were raised as a concern by the community. The analysis determined that the addition of project traffic would not result in any significant impacts. The project does propose relocation of the existing Willow Street bus stop and construction of a new four foot deep bus duckout that would allow eastbound Willow Street traffic to safely pass a parked bus. The bus duckout configuration and shelter design will be finalized at the Planned Development Permit stage to incorporate at a minimum of 10 feet and 15 feet of pedestrian circulation wherever possible along Willow Street.

CONCLUSION

The proposed Planned Development Rezoning will help further the goal of strengthening the Lincoln Avenue Neighborhood Business District by providing a combination of pedestrian friendly commercial,

including office and retail uses. The project will appropriately maximize the use of this infill opportunity site at this key location near transit and shopping while being compatible with the neighborhood context.

PUBLIC OUTREACH/INTEREST

- Criteria 1:** Requires Council action on the use of public funds equal to \$1 million or greater. **(Required: Website Posting)**
- Criteria 2:** Adoption of a new or revised policy that may have implications for public health, safety, quality of life, or financial/economic vitality of the City. **(Required: E-mail and Website Posting)**
- Criteria 3:** Consideration of proposed changes to service delivery, programs, staffing that may have impacts to community services and have been identified by staff, Council or a Community group that requires special outreach. **(Required: E-mail, Website Posting, Community Meetings, Notice in appropriate newspapers)**

Although this item does not meet any of the above criteria, staff followed Council Policy 6-30: Public Outreach Policy. A community meeting was held for the project at the Willow Glen Senior Center on September 24, 2009. Approximately 30 members of the community were present.

As previously indicated, some community members expressed concerns in regards to the project, including; traffic issues on Lincoln Avenue, Willow Street and Blewett Avenue; access; sidewalk widths; visibility of the parking and office structures from the residential property lines; and height – particularly of the tower element. See analysis for information that addresses these issues. The president of the Willow Glen Neighborhood Association, together with several Lincoln Avenue business owners and Willow Glen residents expressed strong support for the project as designed, noting that the character of the architecture suits Willow Glen and that the economic vitality of the community will benefit from the influx of new businesses.

Prior to the filing on the application, the developer had presented his preliminary proposal to the community on December 11, 2008 as part of a Council District 6 roundtable meeting with the City Councilmember.

A notice of the Mitigated Negative Declaration and public hearing was distributed to the owners and tenants of all properties located within 1,000 feet of the project site and posted on the City website. The rezoning was also published in a local newspaper, the Post Record. This staff report is also posted on the City's website. Staff has been available to respond to questions from the public.

Project Manager: Sally Zarnowitz **Approved by:**  **Date:** 1/5/2010

Owner/Applicant: PAJA Investments, c/o Alex Byer 66 Potrero Avenue, San Francisco, CA 94130	Attachments: Development Standards Plans Public Works Memo
Charles Kahn, Kahn Design Associates 1810 6 th Street, Berkeley, CA 94710	

PDC09-016

WILLOW STREET & LINCOLN AVENUE

The following notes are to be incorporated on the final General Development Plan upon City Council Approval. These notes shall replace all other notes currently identified on said plan(s).

ALLOWED USES:

Commercial development range: Up to 58,401 square feet of commercial uses, including: up to 17,258 square feet of ground floor retail uses and up to 26,120 of second and third floor office uses.

Allowed commercial uses for the ground floor shall include all those allowed by right in the CP Commercial Pedestrian and CO Commercial Office Zoning Districts. Second and third floor uses shall be limited to those allowed under the CO Commercial Office Zoning District.

Conditional uses as enumerated in the CP Commercial Pedestrian and/or CO Commercial Office Zoning Districts may be considered and shall require the approval of a Planned Development Permit or Amendment.

DEVELOPMENT STANDARDS:

Setback requirements:

Front setback along Willow Street Front Setback (14' min. of combined public and private sidewalk shall be provided except as noted for Creamery building)	4'*
Historic Creamery Building	0'
Front setback along Lincoln Avenue (Commercial/Mixed Use)	4'
Rear (eastern) setback from adjacent commercial uses	0'
Side and Rear setbacks from residential uses	11'
Tower Roof Peak (over 50' tall) setback from residential uses	200'

Height Requirements:

Maximum height:	50'
Corner Tower Roof Peak	55'

Parking Ratios

Ground floor commercial uses 1:400

Second and third floor office uses 1:250

Environmental Mitigation and Standard Measures:

Mitigation: The BAAQMD has prepared a list of feasible construction dust control measures that the proposed project can implement to reduce construction related air quality impacts to a less-than-significant level. The following mitigation would be implemented during all phases of construction on the project site:

MM AQ1-1: Water all active construction areas at least twice daily or as often as need to control dust emissions.

MM AQ 1-2: Cover all trucks hauling soil, sand, gravel and other loose materials (including demolition debris) and/or ensure that all trucks hauling such materials maintain at least two feet of freeboard.

MM AQ 1-3: Sweep daily or as often as needed with water sweepers all paved access roads, parking areas and staging areas at construction sites to control dust.

MM AQ 1-4: Sweep public streets daily or as often as needed to keep streets free of visible soil material.

MM AQ 1-5: Enclose, cover, water twice daily or apply non-toxic soil binders to exposed stockpiles (dirt, sand, etc.).

MM AQ 1-6: Replant vegetation in disturbed areas as quickly as possible.

MM AQ 1-7: If demolition debris is processed on-site (i.e., ground or crushed), additional dust control measures will be utilized to avoid all visible dust plumes leaving the site.

Mitigation Measures: The following project specific mitigation measures will be implemented during construction to avoid abandonment of raptor and other protected migratory bird's nests:

MM BIO 1-1: Construction shall be scheduled to avoid the nesting season to the extent feasible. The nesting season for most birds, including most raptors, in the San Francisco Bay area extends from February through August.

MM BIO 1-2: If it is not possible to schedule demolition and construction between September and January, then pre-construction surveys for nesting birds shall be completed by a qualified ornithologist to ensure that no nests will be disturbed during project implementation. This survey shall be completed no more than 14 days prior to the initiation of construction activities during the early part of the breeding season (February through April) and no more than 30 days prior to the initiation of these activities

during the late part of the breeding season (May through August). During this survey, the ornithologist will inspect all trees and other possible nesting habitats immediately adjacent to the construction areas for nests. If an active nest is found sufficiently close to work areas to be disturbed by construction, the ornithologist, in consultation with CDFG, will determine the extent of a construction-free buffer zone to be established around the nest, typically 250 feet, to ensure that raptor or migratory bird nests will not be disturbed during project construction.

Standard Measure: The project shall implement the following standard measure to avoid impacts to trees in accordance with City of San José Tree Removal Controls (San José Municipal Code Title 13 Chapter 13.32):

- All trees that are to be removed shall be replaced at the following ratios:

Diameter of tree to be Removed	Native	Non-Native	Minimum Size of Each Replacement Tree
17.8 inches or greater (56.0 Inches Circumference)	5:1 ¹	4:1	24-inch box
12 – 17.8 inches (37.7 – 56.0 Inches Circumference)	3:1	2:1	24-inch box
Less than 12 inches (Less than 37.7 Inches Circumference)	1:1	1:1	15-gallon container

- *It is estimated, based on the site plan, that landscaping proposed by the project will include a sufficient number of trees to offset the loss of trees removed by the project. The species and exact number of trees to be planted on the site and on the street as part of the project will be determined in consultation with the City Arborist and the Department of Planning, Building, and Code Enforcement at the development permit stage. In the event that the project site does not have sufficient area to accommodate the required tree mitigation, one or more of the following measures shall be implemented to the satisfaction of the Director of Planning, Building, and Code Enforcement, at the development permit stage:*
 - *The size of a 15-gallon replacement tree can be increased to 24-inch box and count as two replacement trees.*
 - *An alternative site(s) shall be identified for additional tree planting. Alternative sites may include local parks or schools or installation of trees on adjoining properties for screening purposes to the satisfaction of the Director of Planning, Building, and Code Enforcement.*

¹ X:X = tree replacement to tree loss ratio.

- *A donation of \$300 per mitigation tree to Our City Forest for in-lieu off-site tree planting in the community. These funds shall be used for tree planting and maintenance of planted trees for approximately three years. A donation receipt for off-site tree planting shall be provided to the Planning Project Manager prior to issuance of a development permit.*

Mitigation: The following mitigation measures would be implemented during all phases of construction on the project site:

- MM BIO 2-1:** A certified arborist will establish a tree protection zone for each of the street trees prior to start of construction. No grading, construction, demolition or other work shall occur within the tree protection zone. Any modification to the tree protection zone must be approved and monitored by the consulting arborist.
- MM BIO 2-2:** Prior to issuance of demolition permits, all trees to be retained will be fenced in accordance to the established tree protection zone. Fences shall be 6 ft. chain link or equivalent as approved by the consulting arborist. The fences will remain on-site until all grading and construction is completed.
- MM BIO 2-3:** Any root pruning or canopy pruning required for construction purposes shall receive the prior approval of, and be supervised by, the consulting arborist.
- MM BIO 2-4:** Supplemental irrigation shall be applied as determined by the consulting arborist.
- MM BIO 2-5:** If injury should occur to any tree during construction, work will stop in the area around the tree and the damage shall be evaluated by the consulting arborist so that appropriate treatments can be applied.
- MM BIO 2-6:** No materials or liquids of any kind can be dumped or stored within the designated tree protection zones.

Mitigation and Avoidance Measures: The project proposes to implement the following mitigation and avoidance measures:

- MM ARCH 1-1:** *Site clearing, grading and all other ground disturbing construction activities will be monitored by a qualified archaeologist. If historic/prehistoric artifacts or human remains are discovered during ground disturbing activities, the following measures will be implemented:*
- *In compliance with state law (Section 7050.5 of the Health and Safety Code and Section 5097.94 of the Public Resources Code), in the event human remains are encountered during*

grading and construction, all work within 25 feet of the find will stop and the Santa Clara County Coroner's office will be notified. If the remains are determined to be Native American, the Coroner would notify the Native American Heritage Commission to identify the "Most Likely Descendant" (MLD). The City, in consultation with the MLD, would then prepare a plan for treatment, study and reinterment of the remains.

- *In compliance with state law (Section 7050.5 of the Health and Safety Code and Section 5097.94 of the Public Resources Code), in the event that historical artifacts are found, all work within 50 feet of the find will stop and a qualified archaeologist will examine the find. All significant artifacts and samples recovered during construction would be cataloged and curated by a qualified archaeologist and placed in an appropriate curation facility. The archaeologist must then submit a plan for evaluation of the resource to the City of San José Department of Planning, Building and Code Enforcement for approval. If the evaluation of the resource concludes that the found resource is eligible for the California Register of Historic Resources, a mitigation plan must be submitted to the City of San José Department of Planning, Building and Code Enforcement for approval. The mitigation plan must be completed before earthmoving or construction activities can recommence within the designated resource area.*

Standard Measures: The following standard measures are included in the project as conditions of approval to address the loss of buildings identified as eligible of Structure of Merit status:

- Historic Report and Archival Photo Documentation: 35mm photography of structures and sites eligible for listing on the City of San José Historic Resources Inventory as Structures of Merit. Photographs will consist of selected black and white views of the building to the following standards:
 - Cover Sheet – The documentation shall include a cover sheet identifying the photographer, providing the address of the building, common or historic name of the building, date of construction, date of photographs, and descriptions of the photographs.
 - A bond copy of the Historic Report for the building
 - View – Perspective view: front and other elevations. All photographs shall be composed to give primary consideration to the architectural and/or engineering features of the structure with aesthetic considerations necessary, but secondary.
 - Other Technical Considerations – The camera used must be a 35 mm with black and white film (tri-X, Plus-x, or T-Max is recommended). No soft focus lenses are allowed. Lenses may include normal focus length, wide angle, and telephoto. The photographer has a choice of filters, but use of a polar screen is encouraged.

Sunlight is preferred for exteriors, especially the front façade. Light overcast days, however, may provide more satisfactory lighting for some structures. Finally, all areas of the photograph must be in sharp focus.

- **Submission of Photo Documentation:** Two copies of the documentation, including the original prints and negatives, shall be submitted to the Historic Preservation Officer for approval and distribution to History San José and the Northwest Information Center at Sonoma State University. Digital photos may be provided as a supplement to, but not in place of, the above referenced photo documentation. Digital photography must be recorded on a CD and shall be submitted with the other documentation. The photo documentation shall be accompanied by a transmittal stating that the documentation is submitted in fulfillment of standard measures for the loss of the historic resources which shall be named and the address stated.
- **Professional Qualifications:** The documentation is to be conducted by a qualified consultant meeting the professional qualifications standards of the *Secretary of the Interior's Standards and Guidelines for Archeology and Historic Preservation*.
- **Relocation:** Prior to issuance of Public Works clearance, the structure(s) shall be advertised for relocation and an amount equal to the cost of demolition shall be offered to parties interested in relocation. The project applicant shall provide evidence that an advertisement has been placed in a newspaper of general circulation, posted on a website, and posted at the site for a period of no less than 30 days.
- **Salvage:** Prior to issuance of Public Works clearance, the structure(s) and site shall be retained and made available for salvage to salvage companies facilitating the reuse of historic building materials in conformance with the City's Construction and Demolition Diversion Deposit (CDDD) program.
- The neon signs from the Willow Glen Liquors building will be offered to the San José Historical Museum prior to issuance of demolition permits for the building. If the San José Historical Museum does not take the historical signage, the developer will advertise the availability of the signs to salvage companies facilitating the reuse of historic building materials.

Standard Measures: The following measures have been included as part of the project to avoid impacts resulting from implementation of the project:

- Geologic conditions on the project site will require that the proposed structures be designed and built in conformance with the requirements of the Uniform Building Code for Seismic Zone 4. The potential for geologic and soils impacts resulting from conditions on the site can be mitigated by utilizing standard engineering and construction techniques. With incorporation of these measures the project will not expose people or property to significant impacts associated with the geologic conditions of the site including seismic ground shaking, liquefaction, or expansive soils.

- The applicant shall submit a soil investigation report addressing the potential hazard of liquefaction to the City Geologist for review and approval prior to issuance of a grading permit or Public Works Clearance. The investigation should be consistent with the guidelines published by the State of California (CDMG Special Publication 117) and the Southern California Earthquake Center (“SCEC” report). A recommended depth of 50 feet should be explored and evaluated in the investigation.
- Implement standard grading and best management practices to prevent substantial erosion and siltation during development of the site.

Mitigation and Avoidance Measures: The following measures have been included to reduce potential construction related erosion impacts:

MM GEO1-1: All excavation and grading work will be scheduled in dry weather months or construction sites will be weatherized.

MM GEO 1-2: Stockpiles and excavated soils will be covered with secured tarps or plastic sheeting.

MM GEO1-3: Ditches will be installed, if necessary, to divert runoff around excavations and graded areas.

Mitigation Measures: The project proposes to implement the following mitigation measures:

MM HAZ 1-1: If the USTs are still located on-site, they will be removed in accordance with current regulations. Once the USTs are removed, soil sampling will be completed by qualified personnel to determine if soil contamination has occurred directly under the tanks.

MM HAZ 1-2: If soil contamination is found under the USTs (based on soil sampling done after removal of the USTs), the following measures will be implemented as necessary to remediate contamination issues associated with the USTs.

- A soil management plan (SMP) will be prepared to address the handling of impacted soils during project development. In addition, a health and safety plan (HSP) will be prepared addressing worker safety, including the rationale for selection of personal protective equipment (PPE) for site workers during site development and during any post-development construction activities (such as underground utility repairs, where workers might expose contaminated materials). The HSP will also include a discussion of health

risks associated with the contaminants identified at the site. Furthermore, a Sampling and Analysis plan will be prepared with provisions for collecting post-demolition soil samples in previously inaccessible areas to confirm the extent of soil contamination under the pre-existing buildings. The SMP, HSP, and Sampling and Analysis Plan will be prepared and submitted to the City of San José for approval prior to issuance of grading permits.

- The applicant will have a Remedial Action Work Plan (RAWP) prepared by a qualified hazardous materials consultant prior to issuance of grading permits that will detail the remediation work left to be done on-site and the timing of the work. The RAWP will be submitted to the RWQCB for approval.
- Upon approval of the RAWP by the RWQCB, the RAWP will be submitted to the City of San José Department of Planning, Building and Code Enforcement for review and approval.
- After the RAWP has been fully implemented, appropriate documentation will be submitted to the RWQCB for issuance of a No Further Action letter. A copy of the No Further Action letter must be filed with the City of San José Department of Planning, Building and Code Enforcement prior to issuance of a building permit.

Standard Measures: The project proposes to conform with the following regulatory programs and to implement the following standard measures to reduce impacts due to the presence of ACMs and/or lead-based paint:

- In conformance with state and local laws, a visual inspection/pre-demolition survey, and possible sampling, shall be conducted prior to the demolition of on-site buildings to determine the presence of asbestos-containing materials and/or lead-based paint.
- During demolition activities, all building materials containing lead-based paint shall be removed in accordance with Cal/OSHA Lead in Construction Standard, Title 8, California Code Regulations 1532.1, including employee training, employee air monitoring, and dust control. Any debris or soil containing lead-based paint or coatings would be disposed of at landfills that meet acceptance criteria for the waste being disposed.
- All potentially friable ACMs shall be removed in accordance with NESHAP guidelines prior to building demolition or renovation that may disturb the materials. All demolition activities will be undertaken in accordance with Cal/OSHA standards contained in Title 8 of CCR, Section 1529, to protect workers from asbestos exposure.

- A registered asbestos abatement contractor shall be retained to remove and dispose of ACMs identified in the asbestos survey performed for the site in accordance with the standards stated above.
- Materials containing more than one percent asbestos are also subject to BAAQMD regulations. Removal of materials containing more than one percent asbestos shall be completed in accordance with BAAQMD requirements.

Mitigation Measures: The following mitigation measures, based on RWQCB Best Management Practices and City policy, are included in the project to ensure compliance with NPDES permit requirements to reduce construction-related water quality impacts:

- MM HYD 1-1:** During construction, burlap bags filled with drain rock will be installed around storm drains to route sediment and other debris away from the drains.
- MM HYD 1-2:** During construction, earthmoving or other dust-producing activities will be suspended during periods of high winds.
- MM HYD 1-3:** During construction, all exposed or disturbed soil surfaces will be watered at least twice daily to control dust as necessary.
- MM HYD 1-4:** During construction, stockpiles of soil or other materials that can be blown by the wind will be watered or covered.
- MM HYD 1-5:** During construction, all trucks hauling soil, sand, and other loose materials will be covered and/or all trucks will be required to maintain at least two feet of freeboard.
- MM HYD 1-6:** During construction, all paved access roads, parking areas, staging areas and residential streets adjacent to the construction sites will be swept daily (with water sweepers).
- MM HYD 1-7:** During construction, vegetation in disturbed areas will be replanted as quickly as possible.
- MM HYD 1-8:** Prior to construction grading for the proposed land uses, the applicant will file a “Notice of Intent” (NOI) to comply with the General Permit administered by the Regional Board and will prepare a Stormwater Pollution Prevention Plan (SWPPP) which addresses measures that would be included in the amendment to minimize and control construction and post-construction runoff. The following measures would be included in the SWPPP:
- Preclude non-stormwater discharges to the stormwater system.

- Effective, site-specific Best Management Practices for erosion and sediment control during the construction and post-construction periods.
- Coverage of soil, equipment, and supplies that could contribute non-visible pollution prior to rainfall events or perform monitoring of runoff.
- Perform monitoring of discharges to the stormwater system.

MM HYD 1-9: The developer will submit a copy of the draft SWPPP to the City of San José Department of Public Works for review and approval prior to construction of the project site. The certified SWPPP will be posted at the site and will be updated to reflect current site conditions.

MM HYD 1-10: The developer will comply with the City of San José Grading Ordinance, including erosion and dust control during site preparation and with the City of San José Zoning Ordinance requirements for keeping adjacent streets free of dirt and mud during construction.

MM HYD 1-11: The developer will restrict grading to the dry season (April 15 through October 15) or meet City requirements for grading during the rainy season.

Standard Measures: The following standard measures, based on RWQCB Best Management Practices and City of San José requirements, are included in the proposed project to ensure compliance with NPDES permit requirements to reduce post-construction water quality impacts:

- When the construction phase is complete, a Notice of Termination (NOT) for the General Permit for Construction will be filed with the RWQCB and the City of San José. The NOT will document that all elements of the SWPPP have been executed, construction materials and waste have been properly disposed of, and a post-construction stormwater management plan is in place as described in the SWPPP for the project site.
- All post-construction TCMs will be installed, operated, and maintained by qualified personnel. On-site inlets will be stenciled in conformance with City requirements and cleaned out a minimum of once per year, prior to the wet season.
- The property owner/site manager will keep a maintenance and inspection schedule and record to ensure that the TCMs continue to operate effectively for the life of the project. Copies of the schedule and record must be provided to the City upon request and must be made available for inspection on-site at all times.

Mitigation Measures: The following measures are included in the project to reduce long-term noise impacts:

MM NOI 1-1: The mechanical equipment will be located on the rooftop of the proposed building and be shielded from the residential neighborhood by noise control rooftop screens or a full enclosure.

MM NOI 1-2: The final design of the mechanical equipment plans will be reviewed by an acoustical specialist to verify that the sound of the equipment will be attenuated to below ~~60~~ 55 dBA DNL at the eastern property line. The mechanical equipment plan and report by the acoustical specialist will be submitted to the Director of Planning, Building and Code Enforcement for approval prior to issuance of installation permits.

Standard Measures: The project shall implement the following standard measures:

- Noise-generating activities at the construction site or in areas adjacent to the construction site associated with the project in any way should be restricted to the hours of 7:00 AM and 7:00 PM, Monday through Friday. No construction activities shall occur on weekends or holidays because the project site is within 500 feet of a residential area.
- Equip all internal combustion engine driven equipment with intake and exhaust mufflers which are in good condition and appropriate for the equipment.
- Locate stationary noise generating equipment as far as possible from sensitive receptors when sensitive receptors adjoin or are near a construction project area.
- Utilize “quiet” air compressors and other stationary noise sources where technology exists.
- The contractor shall prepare a detailed construction plan identifying the schedule for major noise-generating construction activities. The construction plan shall identify a procedure for coordination with the adjacent noise sensitive facilities so that construction activities can be scheduled to minimize noise disturbance.
- Designate a “noise disturbance coordinator” who would be responsible for responding to any local complaints about construction noise. The disturbance coordinator would determine the cause of the noise complaints (e.g., beginning work too early, bad muffler, etc.) and institute reasonable measures warranted to correct the problem. A telephone number for the disturbance coordinator would be conspicuously posted at the construction site.
- Control noise from construction workers radios so they are not audible from the eastern property line.

Add key PW items

TO: Sally Zarnowitz
Planning and Building

FROM: Ebrahim Sohrabi
Public Works

**SUBJECT: FINAL RESPONSE TO
DEVELOPMENT APPLICATION**

DATE: 01/06/10

PLANNING NO.: PDC09-016
DESCRIPTION: Planned Development Rezoning from the CP Commercial Zoning District to the CP(PD) Commercial Zoning District to allow 41,457 square feet of mixed-use (office over retail) with attached parking structure on a 2.08 gross acre site
LOCATION: S/E Corner of Lincoln Avenue and Willow Street
P.W. NUMBER: 3-18429

Public Works received revised plans for the subject project on 01/05/09 and submits the following comments and requirements.

Project Conditions:

Public Works Clearance for Building Permit(s) or Map Approval: Prior to the approval of the Tract or Parcel Map (if applicable) by the Director of Public Works, or the issuance of Building permits, whichever occurs first, the applicant will be required to have satisfied all of the following Public Works conditions. The applicant is strongly advised to apply for any necessary Public Works permits prior to applying for Building permits.

1. **Construction Agreement:** The public improvements conditioned as part of this permit require the execution of a Construction Agreement that guarantees the completion of the public improvements to the satisfaction of the Director of Public Works. This agreement includes privately engineered plans, bonds, insurance, a completion deposit, and engineering and inspection fees.
2. **Grading/Geology:**
 - a) A grading permit is required prior to the issuance of a Public Works Clearance.
 - b) If the project proposes to haul more than 10,000 cubic yards of cut/fill to or from the project site, a haul route permit is required. Prior to issuance of a grading permit, contact the Department of Transportation at (408) 535-3850 for more information concerning the requirements for obtaining this permit.
 - c) Because this project involves a land disturbance of one or more acres, the applicant is required to submit a Notice of Intent to the State Water Resources Control Board and to prepare a Storm Water Pollution Prevention Plan (SWPPP) for controlling storm water discharges associated with construction activity.

Copies of these documents must be submitted to the City Project Engineer prior to issuance of a grading permit.

- d) A soils report must be submitted to and accepted by the City prior to the issuance of a grading permit.
3. **Stormwater Runoff Pollution Control Measures:** This project must comply with the City's Post-Construction Urban Runoff Management Policy (Policy 6-29) which requires implementation of Best Management Practices (BMPs) that include site design measures, source controls, and stormwater treatment controls to minimize stormwater pollutant discharges. Post-construction treatment control measures, shown on the project's Stormwater Control Plan, shall meet the numeric sizing design criteria specified in City Policy 6-29.
- a) The project's preliminary Stormwater Control Plan and numeric sizing calculations have been reviewed. At PD stage, submit the final Stormwater Control Plan and numeric sizing calculations to include the following:
 - i) The project site must be 100% treated. Table 3 of the Stormwater Control Plan indicates untreated areas. At PD Permit stage, provide treatment control measure(s) for these areas.
 - ii) The project proposes to use tree credit toward Post-Construction Treatment Control Measure. Trees required for tree removal mitigation, replacement and street trees are not eligible for credit. Additionally, trees planted in pots or containers may not be credited. At the PD Permit stage, show locations of all eligible trees used for stormwater treatment purpose on plan. Further review is required before tree credit can be granted.
 - b) Final inspection and maintenance information on the post-construction treatment control measures must be submitted prior to issuance of a Public Works Clearance.
 - c) A post construction Final Report is required by the Director of Public Works from a Civil Engineer retained by the owner to observe the installation of the BMPs and stating the all post construction storm water pollution control BMPs have been installed as indicated in the approved plans and all significant changes have been reviewed and approved in advance by the Department of Public Works.
4. **Flood: Zone D** The project site is not within a designated Federal Emergency Management Agency (FEMA) 100-year floodplain. Flood zone D is an unstudied area where flood hazards are undetermined, but flooding is possible. There are no City floodplain requirements for zone D.
5. **Sewage Fees:** In accordance with City Ordinance all storm sewer area fees, sanitary sewer connection fees, and sewage treatment plant connection fees, less previous credits, are due and payable.
6. **Street Improvements:**
- a) New raised planters, walls and decorative street lights within public right-of-way will not be allowed. The proposed stamped concrete, bicycle racks and any removable potted plants, if desired, will be acceptable.

- b) Reconstruct City standard curb, gutter, and 10' attached sidewalk with tree wells along both Lincoln Avenue and Willow Street. Dedication of 2.5' along Willow Street will be required.
 - c) Close unused driveway cut(s).
 - d) Proposed driveway widths to be 26'.
 - e) Construct bus duck-out, pad and shelter along Willow Street to the satisfaction of Director of Public Works.
 - f) Upgrade handicap ramp at southeast corner of Lincoln Avenue and Willow Street to meet current ADA standards.
 - g) Dedication and improvement of the public streets to the satisfaction of the Director of Public Works.
 - h) Repair, overlay, or reconstruction of asphalt pavement may be required. The existing pavement will be evaluated with the street improvement plans and any necessary pavement restoration will be included as part of the final street improvement plans.
7. **Electrical:** Existing electroliers along the project frontage will be evaluated at the public improvement stage and any street lighting requirements will be included on the public improvement plans.
8. **Street Trees:**
- a) The locations of the street trees will be determined at the street improvement stage. Street trees shown on this permit are conceptual only.
 - b) Contact the City Arborist at (408) 277-2756 for the designated street tree.
 - c) Install street trees within public right-of-way along entire project street frontage per City standards; refer to the current "Guidelines for Planning, Design, and Construction of City Streetscape Projects". Street trees shall be installed in cut-outs at the back of curb. Obtain a DOT street tree planting permit for any proposed street tree plantings.

Please contact the Project Engineer, Vivian Tom, at (408) 535-6819 if you have any questions.

Ebrahim Sohrabi
Senior Civil Engineer
Development Services Division

TO: Sally Zarnowitz
Planning and Building

FROM: Vivian Tom
Public Works

**SUBJECT: INITIAL RESPONSE TO
DEVELOPMENT APPLICATION**

DATE: 05/13/09

Approved

Date

PLANNING NO.: PDC09-016

DESCRIPTION: Planned Development Rezoning from the CP Commercial Zoning District to the CP(PD) Commercial Zoning District to allow 41,457 square feet of mixed-use (office over retail) with attached parking structure on a 2.08 gross acre site

LOCATION: S/E Corner of Lincoln Avenue and Willow Street

P.W. NUMBER: 3-18429

Public Works received the subject project on 04/27/09 and submits the following comments and requirements. **Upon completion of the Action/Revisions Required items by the applicant, Public Works will forward a Final Memo to the Department of Planning prior to the preparation of the Staff Report for Public Hearing.**

Actions / Revisions Required:

1. **Public Works Development Review Fees:** Additional Public Works Review Fees are due. Prior to the project being cleared for the hearing and approval process, these fees shall be paid to the Development Services Cashier using the attached invoice(s). Additional fees due are as follows:
 - a) A separate Traffic Report fee will be determined by Public Works and forwarded to Planning upon scoping the traffic report. The fee will be required upon release of the workscope for the traffic report.
 - b) An additional NPDES - C.3 Requirements Review Fee of \$1,369.00 is due.

2. **Stormwater Runoff Pollution Control Measures:**
 - a) Revise the Stormwater Control Plan to include the following:
 - i) Provide landscape based treatment control measures such as flow through planter boxes, swales or pervious pavement in combination with the proposed mechanical devices.
 - ii) The preliminary numeric sizing calculations based on the Stormwater Control Plan, prepared by a qualified stormwater professional (civil engineer, licensed architect or landscape architect), used to determine runoff quantity and to design/select the post-construction treatment control measures.

3. **Transportation:** A Traffic Report is required prior to environmental clearance or zoning.
4. **Sanitary:** Submit a conceptive sanitary sewer plan prior to approval.
5. **Storm:** Indicate the overland release path in arrows. The release path shall be paved.
6. **Plan Revisions:** Revise the civil plan as follows:
 - a) Use engineering scale rather than architectural.
 - b) Proposed driveway widths to be 26'.
 - c) Revise cross-sections to show sidewalk dimensions. 10' attached sidewalk is required on both Willow Street and Lincoln Avenue. Indicate amount of dedication necessary to accomplish the required sidewalk width.
 - d) This plan has been forwarded to the VTA for their comments. Installation of a bus duck-out or relocation of this bus stop may be required.
 - e) Due to maintenance, liability and consistency within the neighborhood, all proposed non City standard features will not be allowed. Revise the plan to eliminate all decorative lighting, special pavement, planters and walls in the public right-of-way.

Project Conditions:

Public Works Clearance for Building Permit(s) or Map Approval: Prior to the approval of the Tract or Parcel Map (if applicable) by the Director of Public Works, or the issuance of Building permits, whichever occurs first, the applicant will be required to have satisfied all of the following Public Works conditions. The applicant is strongly advised to apply for any necessary Public Works permits prior to applying for Building permits.

7. **Construction Agreement:** The public improvements conditioned as part of this permit require the execution of a Construction Agreement that guarantees the completion of the public improvements to the satisfaction of the Director of Public Works. This agreement includes privately engineered plans, bonds, insurance, a completion deposit, and engineering and inspection fees.
8. **Grading/Geology:**
 - a) A grading permit is required prior to the issuance of a Public Works Clearance.
 - b) If the project proposes to haul more than 10,000 cubic yards of cut/fill to or from the project site, a haul route permit is required. Prior to issuance of a grading permit, contact the Department of Transportation at (408) 535-3850 for more information concerning the requirements for obtaining this permit.
 - c) Because this project involves a land disturbance of one or more acres, the applicant is required to submit a Notice of Intent to the State Water Resources Control Board and to prepare a Storm Water Pollution Prevention Plan (SWPPP) for controlling storm water discharges associated with construction activity. Copies of these documents must be submitted to the City Project Engineer prior to issuance of a grading permit.
 - d) A soils report must be submitted to and accepted by the City prior to the issuance of a grading permit.

9. **Stormwater Runoff Pollution Control Measures:** This project must comply with the City's Post-Construction Urban Runoff Management Policy (Policy 6-29) which requires implementation of Best Management Practices (BMPs) that include site design measures, source controls, and stormwater treatment controls to minimize stormwater pollutant discharges. Post-construction treatment control measures, shown on the project's Stormwater Control Plan, shall meet the numeric sizing design criteria specified in City Policy 6-29.
10. **Flood: Zone D**
 - a) The project site is not within a designated Federal Emergency Management Agency (FEMA) 100-year floodplain. Flood zone D is an unstudied area where flood hazards are undetermined, but flooding is possible. There are no City floodplain requirements for zone D.
11. **Sewage Fees:** In accordance with City Ordinance all storm sewer area fees, sanitary sewer connection fees, and sewage treatment plant connection fees, less previous credits, are due and payable.
12. **Street Improvements:**
 - a) Reconstruct City standard curb, gutter, and 10' attached sidewalk with tree wells along both Lincoln Avenue and Willow Street. Dedication may be required.
 - b) Close unused driveway cut(s).
 - c) Proposed driveway widths to be 26'.
 - d) Upgrade handicap ramp at southeast corner of Lincoln Avenue and Willow Street to meet current ADA standards.
 - e) Dedication and improvement of the public streets to the satisfaction of the Director of Public Works.
 - f) Repair, overlay, or reconstruction of asphalt pavement may be required. The existing pavement will be evaluated with the street improvement plans and any necessary pavement restoration will be included as part of the final street improvement plans.
13. **Complexity Surcharge:** Based on established criteria, the public improvements associated with this project have been rated medium complexity. An additional surcharge of 25% will be added to the Engineering & Inspection (E&I) fee collected at the street improvement stage.
14. **Electrical:** Existing electroliers along the project frontage will be evaluated at the public improvement stage and any street lighting requirements will be included on the public improvement plans.
15. **Street Trees:**
 - a) The locations of the street trees will be determined at the street improvement stage. Street trees shown on this permit are conceptual only.
 - b) Contact the City Arborist at (408) 277-2756 for the designated street tree.

- c) Install street trees within public right-of-way along entire project street frontage per City standards; refer to the current “Guidelines for Planning, Design, and Construction of City Streetscape Projects”. Street trees shall be installed in cut-outs at the back of curb. Obtain a DOT street tree planting permit for any proposed street tree plantings.
 - d) Replace any missing street trees in empty tree wells or park strips along Lincoln Avenue and Willow Street and match existing trees per City standards; refer to the current “Guidelines for Planning, Design, and Construction of City Streetscape Projects”. Obtain a DOT street tree planting permit for any proposed street tree plantings.
 - e) Show all existing trees by species and diameter that are to be retained or removed. Obtain a street tree removal permit for any street trees that are over 6 feet in height that are proposed to be removed.
16. **Referrals:** This project should be referred to the Santa Clara Valley Transportation Authority (VTA).

Please contact me at (408)535-6819 or Joshua Kenton at (408) 535-6810 if you have any questions.

Vivian Tom
Project Engineer
Development Services Division

VT:jk
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The 1104 Lincoln Avenue Mixed-use Project (PDC09-016) Initial Study was circulated to affected public agencies and interested parties for a 22-day review period from December 22 to January 12, 2010. This memo responds to comments received by the Lead Agency on the Initial Study during the public review period.

Five comment letters were received during the designated review period. The comment letters are presented below, in the order received, with responses from the Lead Agency. When a single issue was raised by more than one commentator, a master response was prepared.

Master Response 1:

Increased Traffic on Blewett Avenue

The San José Department of Transportation has worked with the neighborhood to address existing conditions such as cut-through traffic and speeding on residential streets surrounding Lincoln Avenue. As a result of this coordination, speed bumps were installed on Blewett Avenue to discourage non-residents from using the street as a bypass for congested areas on Lincoln Avenue and Willow Street. Currently, the traffic volumes on Blewett Avenue are approximately 950 daily traffic trips, which is within an acceptable volume range for a local residential street in San José. Implementation of the proposed project will not significantly increase the traffic on Blewett Avenue because of the minimal increase in Peak Hour (i.e., rush hour) traffic expected on Willow Street (as shown on Figure 8-10 of Appendix F of the Initial Study) and the existing speed bumps on Blewett make this roadway a slow and undesirable alternative route.

A transportation impact analysis (TIA) was prepared for the proposed project consistent with the methodology established by the Santa Clara Valley Transportation Authority (VTA) and City of San José for the analysis of traffic impacts. Traffic conditions and the functionality of the existing roadway system were analyzed based on the level of service (LOS) of signalized intersections in the project area. The VTA and City of San José adopted methodology does not analyze individual roadway segments as a function of roadway capacity. As discussed on page 96 of the Initial Study, the additional project traffic would not impact any signalized intersection in the project area and, therefore, the project was determined to have a less than significant traffic impact. As a result, no mitigation is required or proposed as part of the project.

Master Response 2:

Setback of Sound Wall

The proposed sound wall on the ramp and parking deck will be set back a minimum of 11 feet from the eastern property line.

Master Response 3:

Length of Review for the Initial Study

Section 15105 of the CEQA Guidelines states that “the public review period for a proposed negative declaration or mitigated negative declaration shall be not less than 20 days.” CEQA review periods are based on calendar days regardless of weekends or holidays. The City circulated the document for 22 days which meets the legal requirements of CEQA. The document was available for review on the City’s website during the published review period so the closure of City offices did not preclude the review of the document during the designated review period.

Comment Letter 1: Todd Weber and Linda Kerr, January 3, 2010.

Comment 1-1: My wife and I live at 1134 Blewett Avenue. Here are our comments regarding the Willow/Lincoln Development PDC09-016:

We do not understand why there is no discussion in any of the preliminary nor final reports as to the impact that this development will have on traffic on Blewett Avenue. With the amount of regular traffic on Willow Street each day and throughout the evening, vehicles leaving the development's parking area via the Willow Street exit will most likely take a right turn onto Willow, then right again at Blewett – this will likely add significant cut-through traffic onto Blewett yet there is no mention or consideration of that impact into the Blewett residential area. How and when will that traffic issue be considered and a mitigation proposed or required of the developer?

Response 1-1: Please see Master Response 1.

Comment 1-2: There is a disconnect within the Preliminary (pg. 68 @ 10 feet) and Final (pg. 2 @ 11.5 feet) Reports as to the setback of the sound wall along the parking ramp structure. Shouldn't it be corrected for both to show whichever is the appropriate setback distance?

Response 1-2: Please see Master Response 2.

Comment 1-3: It seems inappropriate to have compressed the community review and comment period by including the Holiday period while the City has been basically shut down.

Response 1-3: Please see Master Response 3.

Comment Letter 2: Chris and Raquel Dominguez, January 6, 2010.

Comment 2-1: Hello, we own the residence at 1165 Blewett Avenue. Here are our comments regarding the Willow/Lincoln Development PDC09-016.

We believe if properly constructed to mitigate the negative impact on the surrounding single family residences it will make for a better Willow Glen. (still do not understand why we need a multi story building here)

Response 2-1: This comment is noted.

Comment 2-2: This puts our home, backyard with in clear view of any business/building higher than 1 story.

Response 2-2: The commentors property is located at the southeast corner of the project site. As shown on Figure 7 (page 10) of the Initial Study, this property would be in line with the surface parking lot proposed for the project. With implementation of the proposed project, the nearest commercial building to this residential property will be the existing Jamba Juice/ Noah's Bagels building located approximately 95 feet west of the property line. The proposed commercial building will be approximately 275 feet from the northwest corner of the commentors property. Given this setback and existing mature trees along the property line, there would be enough visual separation to avoid privacy and visual intrusion between the commercial building and backyard areas.

Comment 2-3: We will be impacted by the extra noise from vehicles above any walls and the HVAC noise which can clearly carry across the open space due to poor tree selection.

Response 2-3: As stated in the Project Description and discussed in Section 4.11 of the Initial Study, the project proposed a five foot sound wall along the ramp and southeast corner of the parking deck. The five foot wall is consistent with the recommendations of the noise analysis and will attenuate noise from the parking deck to an acceptable level. As a result, the ambient noise level in the residential neighborhood will not noticeably increase as a result of the proposed parking deck. At the request of the neighborhood, a stepped, eight foot wall may replace the proposed five foot wall along the ramp to the parking deck. An 8-foot wall will exceed the recommendations for noise attenuation and result in the same or greater mitigation of noise from the parking deck as the originally proposed five foot wall.

Section 4.11.2.2 – *Mechanical Equipment Related Noise* on page 75 of the Initial Study discusses the impact of mechanical equipment on the roof of the proposed building and identifies mitigation measures to reduce the impact to a less than significant level. With implementation of the proposed mitigation, the development will be consistent with City noise ordinance regarding the noise levels of mechanical equipment.

Comment 2-4: So far Alex Byer has made poor choices to isolate the existing businesses behind our home...thin pine trees instead of London Plane (Sycamores) which would be more effective against noise/sight and more uniform to the majority of trees found within the Willow Glen area. Additionally the misplacement of constructing refuse/garbage receptacles to the rear of houses on the south side of their existing commercial property, as opposed to the far north side where such receptacle areas would be adjacent to existing business and demonstrate that they are good neighbors.

Response 2-4: The issues raised in this comment pertain to existing conditions/operational issues of the project. This comment does not raise any environmental concerns regarding the proposed project and no response is required. These comments will, however, be included in the staff report to Planning Commission and City Council.

Comment 2-5: We do not understand why there is no discussion in any of the preliminary nor final report as to the impact that his development will have on traffic on Blewett Avenue. With the amount of regular traffic on Willow Street each day and throughout the evening, vehicles leaving the development's parking area via the Willow Street exit will most likely take a right turn onto Willow, then right again at Blewett – this will likely add significant cut-through traffic onto Blewett yet there is no mention or consideration of that impact into the Blewett residential area. How and when will that traffic issue be considered and a mitigation proposed or required of the developer?

Response 2-5: Please see Master Response 1.

Comment 2-6: There is a disconnect within the Preliminary (pg. 68 @ 10 feet) and Final (pg. 2@ 11.5 feet) Reports as to the setback of the sound wall along the parking ramp structure. Shouldn't it be corrected for both to show whichever is the appropriate setback distance?

Response 2-6: Please see Master Response 2.

Comment 2-7: It seems inappropriate to have compressed the community review and comment period by including the Holiday period while the City has been basically shut down.

Response 2-7: Please see Master Response 3.

Comment Letter 3: Lynn Repetsky and Mark Cooney, January 7, 2010.

Comment 3-1: Following are our comments regarding the Willow/Lincoln Development PDC09-016.

We are very concerned that the impact of traffic on Blewett Avenue is not mentioned in the preliminary nor final report. Since Willow is such a busy street, vehicles exiting on Willow Avenue will need to turn right out of the parking area. If they proceed south, they will most likely turn on to Blewett. We feel that the impact on this residential street needs to be considered and mitigation proposed.

Response 3-1: Please see Master Response 1.

Comment 3-2: Additionally we feel it was inappropriate to have compressed the community review and comment period during the holidays which included a period when the city was shut down.

Response 3-2: Please see Master Response 3.

Comment Letter 4: Chris and Ellen Rowe, January 8, 2010.

Comment 4-1: As you may know, we live at 1155 Blewett, directly behind the proposed parking ramp/structure of the proposed development. As you also know Councilman Oliverio, we initially had a discussion (via an email sent directly to you and calls to your office) with one of your representatives regarding the proposed development about a year ago.

We have also attended “neighborhood meetings” including WGNA meetings and Blewett Neighborhood Group meetings, etc.

We understand that there will be to review meetings in the coming weeks and we plan to attend and speak at both.

As we have previously noted, we are concerned about several elements of the development. We have had the opportunity to have some direct input to the architect about some of our most worrisome concerns...specifically the sound/privacy wall and its many aspects. The current issues include:

A wall height that ensures our privacy and does not allow either pedestrians or passengers in cars/trucks to see into our backyard.

A wall that blocks the sound that will be emitted from the parking structure and specifically the ramp itself.

Response 4-1: As stated in the Project Description and discussed in Sections 4.9 and 4.11 of the Initial Study, the project proposes a five foot sound wall along the ramp and southeast corner of the parking deck. This wall would be a minimum of 11 feet from the property line. Because of the setback from the property line and the existing mature trees, there will be enough visual separation between the houses and the parking deck to avoid privacy issues.

The five foot wall is consistent with the recommendations of the noise analysis and will attenuate noise from the parking deck to an acceptable level. As a result, the ambient noise level in the residential neighborhood will not noticeably increase as a result of the proposed parking deck.

At the request of the neighborhood, the five foot wall may be replaced with a six foot wall on the parking deck and a stepped, eight foot wall along the ramp to the parking deck. These new wall heights will exceed the recommendations for noise attenuation and result in the same or greater mitigation of noise from the parking deck as the originally proposed five foot wall. In addition, the greater wall height will provide additional visual separation between the residences and the parking deck.

Comment 4-2: The distance of the wall from our property line. We understand that this type of development has a minimum 15' set-back from adjoining residential property lines. We understand that this rule will be broken and the distance will be reduced to 11'6". There needs to be close analysis of distance and why a regulation will be potentially overruled and a reduction granted.

Response 4-2: The project proposes a Planned Development (PD) Zoning which allows the City to define the parameters of the development. Land use compatibility issues were analyzed in Section 4.9 of the Initial Study. No environmental impact was identified due to the proposed reduced setback.

Comment 4-3: The overall appearance of the wall and how it impacts our backyards i.e. appearance and highly increased shade that could kill existing trees and landscaping.

Response 4-3: The existing cinderblock wall that separates the residences from the existing parking lot on the project site will not be modified or replaced. The new wall will be located on the ramp of the parking deck and along the southeast corner of the upper parking deck. This wall was proposed to be five feet tall and located a minimum of 11 feet away from the property line. At the request of the neighborhood, the five foot wall may be replaced with a six foot wall on the parking deck and a stepped, eight foot wall along the ramp to the parking deck. The new wall will not result in an increase in shading that would adversely effect landscaping on the residential properties as long as it does not exceed the new wall heights proposed by the neighborhood. As discussed in Section 4.9.2.1 of the Initial Study, existing mature trees along the property line will screen or soften views of the proposed wall from the residential backyards. While the residents of this property may have some views of the proposed development from their back yard, private views are not protected scenic resources. It is not a significant environmental impact for a structure to be visible in an existing urban setting.

Comment 4-4: Other issues include, the overall noise of the many aspects of the development, especially as it relates to delivery trucks going up the ramp along with the placement of trash receptacles. We currently deal with very load dumpster truck operations at very early morning and late evening hours, and we deal with deliveries at various hours – permitted delivery schedules (6am to 11pm) are regularly abused. This has been reported to the appropriate city department several times (progress is initially made with calls from the city but is then later abused again).

Alex Byer, and his architect have personally and face to face ensured us that they will “do whatever it takes to make it right”, and “listen and address our every concern.” And to his credit, Charles Kahn, the developer’s architect, has created a wall (per the latest plan he has shown us) that seems to address our many concerns with privacy. However, we want to make sure that these plans are not manipulated at a later date to negatively affect us. It’s crucial that the utmost concern be considered

in respect to the issues associated with the residential neighbors and ensure we closely analyze each and every design impact.

Response 4-4: The issues raised in this comment pertain to existing conditions, operational issues of the project, or procedural issues with the City's development process. This comment does not raise any environmental concerns about the project and no response is required. These comments will, however, be included in the staff report to Planning Commission and City Council.

Comment 4-5: As an additional point, we also are very concerned about the construction process. We understand there was considerable vibration, dust, noise and traffic when the Jamba Juice/Noah's Bagels building went in. This is obviously a much bigger development and will pose bigger challenges.

Besides our privacy, we are also very concerned how the loud noises due to demolition and construction will affect our 16-month old son's health, hearing, and quality of life. He is cared for in our home and plays in our backyard on a daily basis. We want you to explore how these construction concerns and its potential implications on his health and quality of life can be mitigation during the life of the construction.

Response 4-5: Section 4.11 of the Initial Study analyzed both the operational and construction noise impacts of the proposed project. The second paragraph under Section 4.11.2.2 – *Construction Related Noise* on page 76 of the Initial Study specifically discusses the impact of construction noise on the four residences that abut the project site. The report states that the existing ambient noise levels at the adjacent residences are approximately 63 dBA DNL. Construction-related noise levels at these residences would intermittently exceed 65 dBA L_{eq} and existing ambient levels would increase by more than 5 dBA L_{eq} when construction occurs on-site.

The project will be required to implement standard noise control measures for the duration of project construction including limits on construction hours, mufflers on internal combustion engine equipment, preparation of a construction management plan, designation of a noise disturbance coordinator, and other measures. Implementation of these measures will reduce construction-related noise impacts to a less than significant level. It is possible, however, that a perceived nuisance would still occur, but that is typical of any construction near a residential neighborhood.

Comment 4-6: We are also very concerned about traffic. It is readily apparent that traffic is already at a fragile balance at the Lincoln Willow intersection. We already have heavy traffic/backup situations during morning rush hours and evenings from rush hour through later evening restaurant traffic. There are so many elements to the traffic situation including further impact during the mornings on Blewett Avenue. There is often a back up down Blewett from cars wanting to turn left of Blewett onto Willow. These cars on Blewett cannot turn left since cars are backed up from the Willow light at Lincoln down Willow past Blewett Avenue. This very traffic jam also happens during rush hours in the evening. Unfortunately, this also happen on the north side of Willow as cars exit out of the Aquí/Blockbuster/Beverages and More parking lot – traffic backs up past this exit/entrance and causes huge backups in that development's parking lot. These current stresses on traffic have been recognized by the city and they have at times had police officers directing traffic at the intersection. This intersection will surely become much worse and a traffic flow program has to be closely analyzed. As noted by several people, there will be increased cut-through traffic on Blewett (Councilman Oliverio you are readily aware of the traffic issues on Blewett). The

development could very well be or become underparked and the already stressed situation of cars parking Blewett, especially from retail uses, will become a major issue.

Response 4-6: Please see Master Response 1 for the traffic issues.

The project proposes a total of 157 parking spaces consistent with City requirements.

Comment 4-7: On a related issue, we do not want to see the bus stop that is currently in front of the Willow Glen Yogurt Shop moved near the Blewett Avenue corner. This will affect in and out traffic flow on Blewett and create added noise, ruff raff, and blown fumes that will impact Blewett residents who live on the north end of Blewett Avenue.

Response 4-7: Based on the site plan (page 7 of the Initial Study), the existing bus stop on Willow Street will be relocated approximately 35 feet (approximately 190 feet from the intersection of Willow Street and Blewett Avenue). Adjacent to the relocated bus stop, the project will be constructing a bus duck out. The new duck out will improve site distance at the project driveway on Willow Street and facilitate left-turns. It will also allow the bus to move out of the traffic lane creating an unimpeded eastbound travel lane. By moving the buses out of the travel lane, the intersection of Lincoln Avenue and Willow Street will not be obstructed when a bus is parked along Willow Street. By allowing traffic to flow more freely on Willow Street, there will be less back up onto residential streets, including Blewett. The relocation of the bus stop approximately 35 feet east will not increase the noise, pedestrian traffic, or diesel exhaust that residents of Blewett Avenue currently experience from the existing bus stop.

Comment Letter 5: Chris and Raquel Dominguez, January 10, 2010.

Comment 5-1: To ensure that our voice / input does not get lost in the shuffle especially since I have never received any notice from the City of San Jose regarding this development which will place a 2 story parking garage ten to eleven feet from my property line and approximately twenty-five feet at most from the master bedroom slider, I though I might be consulted/ involved/informed about this development. I am not opposed to development, though it should not completely infringe upon one's existing home. I think this project can and will be successful with some significant fine-tuning.

Additionally, I have contacted the developer and have not heard back from him, (though I understand he selectively met with some of the property owners.)

I know that the existing 8 foot high sound wall and single pine tree will not block / alleviate the noise consideration when the wall and tree do not block / alleviate the current noise from vehicular traffic and the garbage truck which arrives every morning at 6:00 am. (We have never complained about this because these issues were existing when we purchased the property.) The new issues from noise, traffic headlights, parking garage lights, visible parking garage in our backyard, and reduced sun light are an entirely new matter. We in no way compare this to a two story single family residence, because there would be (in Willow Glen) a greater setback then 10-11.5 feet from the property line as discussed in the plan.

I do not think the Land use impact, nor noise impact in anyway represent the intrusiveness of the project upon the existing homeowners; see below.

4.9.2.1 Land Use Impacts

The proposed parking deck could also be perceived as a visual intrusion conflict. The deck would be 15 feet above the ground surface, which is seven feet higher than the existing cinderblock wall. The parking deck will be setback 10 feet from the eastern property line and the existing mature trees along that property line will provide visual separation between the houses and the parking deck. In addition, the project proposes a five to six foot tall solid wall along the eastern edge of the parking deck which would further reduce the visibility of the adjacent residential properties. Therefore, the proposed parking deck will not result in a visual intrusion impact on the adjacent residences.

4.11.2.2 Noise Impacts from the Project

Parking Deck Related Noise

The project proposes to reconfigure the existing parking lots on the project site into a single surface lot with an elevated parking deck. Typical parking lot activities generate maximum noise levels of 50 to 60 dBA Lmax when measured at 50 feet from the source of the noise. Car alarms can generate maximum noise levels of 70 dBA Lmax. The nearby residences, which are adjacent to the parking lot, would experience noise levels of 50 to 60 dBA Lmax in the outdoor use areas from the surface lot. The parking deck is expected to generate noise in excess of 60 dBA. The noise analysis concluded that the noise from the parking deck could be attenuated to an acceptable level with the inclusion of a 42-inch solid wall along the eastern edge of the parking deck and access ramp. The project proposes to construct a five to six foot tall solid wall along the eastern perimeter of the parking deck and access ramp. Therefore, the parking deck will have a less than significant noise impact on the adjacent residences.

I am concerned about the fairness of this project and the ability to which Council wants to work with the property owners when I have never been contacted regarding the project. I have been told that Councilmember Oliverio stands 100% behind this project and wants it to come to fruition at any cost. I do not know differently because I have not been contacted / communicated with about the project.

Response 5-1: The commentor's concerns regarding land use compatibility are acknowledged. The commentor's property (1165 Blewett Avenue) is located at the southeast corner of the project site. As shown on Figure 7 (page 10) of the Initial Study, this property would be in line with the surface parking lot proposed for the project. While this property does not have as many mature trees along the shared property line as the other three houses that abut the project site, the proposed ramp and parking deck are not located near this residence. With the location of the ramp and parking deck in relation to the subject residential property, there will be no visual intrusion or loss of privacy as a result of the proposed project.

The issues regarding traffic noise and traffic headlights will be mitigated by the wall proposed on the ramp and parking deck. Parking garage lights will comply with City requirements and will not increase the ambient light levels in the project area. Due to the location of the commentor's property in relation to the parking deck, the parking deck will not be visible from the backyard and there is no structure proposed within close proximity to this residential property that would result in substantial new shading or a reduction in access to sunlight.

Zarnowitz, Sally

From: todd Weber [toddnlil@mac.com]
Sent: Sunday, January 03, 2010 10:51 PM
To: Zarnowitz, Sally
Cc: todd Weber
Subject: Willow/Lincoln Development PDC09-016

Ms. Zarnowitz,

My wife and I live at 1134 Blewett Avenue. Here are our comments regarding the Willow/Lincoln Development PDC09-016:

- 1) We do not understand why there is no discussion in any of the preliminary nor final report as to the impact that this development will have on traffic on Blewett Avenue. With the amount of regular traffic on Willow Street each day and throughout the evening, vehicles leaving the development's parking area via the Willow Street exit will most likely take a right turn onto Willow, then right again at Blewett - this will likely add significant cut-through traffic onto Blewett yet there is no mention or consideration of that impact into the Blewett residential area. How and when will that traffic issue be considered and a mitigation proposed or required of the developer?
- 2) There is a disconnect within the Preliminary (pg. 68 @ 10 feet) and Final (pg. 2 @ 11.5 feet) Reports as to the setback of the sound wall along the parking ramp structure. Shouldn't it be corrected for both to show whichever is the appropriate setback distance?
- 3) It seems inappropriate to have compressed the community review and comment period by including the Holiday period while the City has been basically shut down.

Sincerely,
Todd A. Weber and Linda L. Kerr
toddnlil@mac.com
1134 Blewett Ave.
San Jose, CA 95125

Zarnowitz, Sally

From: Andrea Wheeler [awheeler@demoworks.com]

Sent: Thursday, December 24, 2009 3:50 PM

To: Zarnowitz, Sally

Subject: re: PDC09-016

Hello,

I am writing about the planned development at the corner of Willow and Lincoln. I fully support this project with two concerns: 1) adequate parking is provided and 2) there are adequate entrances and exits, particularly addressing left hand turns.

Thanks,

Andrea Wheeler

1265 Kottenberg Ave

San Jose CA 95125

Zarnowitz, Sally

From: Zarnowitz, Sally
Sent: Tuesday, September 29, 2009 11:47 AM
To: 'JeanAnn2@aol.com'
Subject: RE: LOS Lincoln/Willow

Thank you for your comments.
This message will be placed in the project file.

Sally Notthoff Zarnowitz AIA LEED AP
voice 408. 535. 7834

-----Original Message-----

From: JeanAnn2@aol.com [mailto:JeanAnn2@aol.com]
Sent: Monday, September 28, 2009 10:50 AM
To: Zarnowitz, Sally
Cc: Enderby, Mike
Subject: LOS Lincoln/Willow

Hi Sally,
As I mentioned, I did some research last summer on the LOS and "protected intersection" status of Lincoln and Willow. I decided to research after a Willow Glen elist dialogue that included comments from a local resident who indicated she was very familiar with the Lincoln/Willow traffic study--prior to its release.

In any case, I found Resolution 72765.2 from June 21, 2005 which "protected" ten intersections outside of downtown core and specific area plans. I have attached. The EIR studied 13 intersections, but Director Haus' memo highlighted that three of the intersections were recommended for dropping. Lincoln and Willow was one of the three dropped interesection. Council minutes list the ten approved intersections.

Apparently, some folks are using the longer list because the EIR is readily available. There is even a map floating around.

I researched subsequent council minutes and couldn't find an additions. I asked the Clerk's office to look for modifications to Resolution 72765.2 and they didn't find any. After the signature are the online references. I checked them again this morning and they are all active.

Have you found something later than June 21, 2005 that "protects" Lincoln/Willow? If not, then this intersection is NOT a protected intersection and traffic improvements are appropriate--such as a 5 foot cut-out for a bus.

As an aside, many people expressed concern about "smelly buses" near the plaza. Perhaps, VTA would be willing to make committments about using their alternate fuel buses on this route 25. All of their buses are expected to be converted in the next ten years. It seems stupid to design for "smelly buses" when they are due to be phased out.

Best,
Jean Dresden

Resources:

Council Minutes Item 12.3c
http://www.sanjoseca.gov/clerk/2005_CnclMins/062105Min.pdf

Resolution 72765.2
June 21, 2005
Initial list of ten outside of downtown core and the total list

Staff memo

13 intersections studied in EIR

ten recommended

http://www.sanjoseca.gov/clerk/Agenda/06_21_05docs/062105_12.03c.pdf

"Democracy is a team sport. It is not like going to a ballgame where you sit passively and decide if you like the players and evaluate their abilities after watching the game. We are the players, we are the team, in a democratic nation or a democratic world."

-John Renesch

</HTML>

Zarnowitz, Sally

From: Hugh Graham [hguh1761@earthlink.net]
Sent: Friday, September 25, 2009 4:37 PM
To: Zarnowitz, Sally
Subject: PDC09-016 & 018

Sally,

I was in attendance at both last night's meeting and the one in November. I am most impressed with the design quality of this project and the significant effort by the applicant to meet community concerns. I believe the project as proposed can make a significant positive impact on Willow Glen and San Jose.

The proposed approach to the bus stop seems appropriate from both a safety aspect and the optimal community use of the patio areas.

Sincerely,

Hugh Graham

--

Zarnowitz, Sally

From: Jo Ann Lawlor [jal_573@yahoo.com]
Sent: Friday, September 25, 2009 1:59 PM
To: Zarnowitz, Sally
Subject: Lincoln Ave. development PDC09-016/PD09-018

Since the meeting on this was just last night, I hope that the issue isn't decided yet--and I hope it gets voted down. Leaving aside the article in Wednesday's paper about all of the unsold housing units downtown, I can think of at least three developments within about five miles of this corner that are also largely unsold. The most egregious example is that mess at Hacienda Gardens. It took forever to complete, judging by the number of empty windows most of the units are still not sold--and no wonder, when about half of the actual shopping center across the street still looks like a war zone. As for the retail part of the Lincoln Ave. project, anyone who has walked up and down Lincoln knows that there is plenty of available retail space without kicking out three businesses that happen to actually be thriving to build more vacant retail space. And then, of course, there's the fact that the Lincoln/Willow intersection is already inadequate for the volume of traffic it handles. In the extremely unlikely case that when they were finally done, all of these units actually filled up, traffic nightmare wouldn't begin to describe it. Hacienda Gardens already stands as a monument to a landlord's greed and a city's indulgence of it, and the individual business owners who were forced out of viable businesses and in some cases, out of business entirely, are enough. We don't need any more. Don't do it--please.

Jo Ann Lawlor
1249 Glenn Ave.SJ 95125
408-287-5164



MEETING NOTES

Meeting Date: September 24, 2009

Meeting Location: Willow Glen Senior Center

Meeting Subject: Community Meeting: Proposed Planned Development & Planning Development Permit
Project site: 1104 Lincoln Avenue

Speakers: Councilmember Pierluigi Oliverio (CPO), Sally Zarnowitz (SZ), Mike Enderby (ME), Alex Byer (AB), Charles Kahn (CK), Community Member (CM)

Item No.	Action/Decision Pending/Follow up
1. Welcome remarks by CPO.	
2. Introduction from SZ. Summary of Planned Development Rezoning, Environmental Review and Planned Development Permit process. CM question regarding special rezoning fielded by ME. ME also provided more explanation of the public input process. CM Question: Can we talk about traffic issues? ME Answer: Yes, now that we have a traffic report to review. If it's determined the project would impact traffic, mitigation measures might be required. CM Question: Is this (Willow & Lincoln) a protected intersection? Concerned about further deterioration. SZ Answer: It will be addressed in the Environmental Report.	
3. Presentation by property owner Alex Byer: <ul style="list-style-type: none"> • Plans to build and hold the property for many years. • Wants to build what is right for the area. • Did extensive research on other high quality commercial centers. • Will continue to work to accommodate existing tenants (Willow St. Pizza and Willow Glen Yogurt Company). 	
4. Presentation by project architect Charles Kahn: <ul style="list-style-type: none"> • Site description: plazas as public amenities, building mass placed on north side to reduce impact on neighbors. • Building description: articulated and stepped back to reduce mass, contemporary Spanish style, changes made to increase prominence of Willow Glen Pizza building. • Parking deck & privacy mitigations for neighbors. • Proposed relocation of bus stop to help traffic flow at corner. • Reduction of curb cuts and other amenities for pedestrians. • Offices for small local businesses. • Sustainable goals: the project is expected to achieve LEED Silver rating. 	

5. Question & Answer Period

CM Question: What do you mean by a new crosswalk?

CK: A change in the paving materials.

CM Question: What other portions of the street are 3 stories?

CK: Predominantly two stories -- there is a 3 story property across the street.

CM Question: Changes from last meeting?

CK: We can make the meeting notes available. We've addressed the bus stop, and can now confirm that solar panels will be in the project.

ME: Some of the changes involved tweaking of the massing along the street. The most significant change was made in the back to the ramp.

CM Question: Can you talk about ingress / egress and location of service areas?

CK: We tried to give people options to avoid the awful intersection. The service areas are still being worked out. We may relocate them to reduce sound for the neighbors on Blewett.

CM Question: I think you clearly listened to people. What will the quality of the materials be?

CK: Stucco with Spanish tile roof, consistent with Mediterranean style. We'll use high quality lighting, tile, etc.

CM Question: Can you provide more details on the Willow Street Pizza building? Which older buildings will be in the Environmental Report?

SZ: The report is not done yet but will look at the ages and significance of all the buildings.

CK: We've tried to reduce the impacts on the Willow Street Pizza building by pulling the new building back around it. We've also enlarged the terrace. We're trying to honor it -- it's a very important building.

CM Question: Will the other buildings be demolished?

CK: We're trying to find a home for the real estate building.

CM Question: How will you prevent graffiti on the rear wall near the parking structure?

CK: There will be no access to the area.

ME: We're going to make the rear setback work to allow trees in front of the wall.

CM Question: What's the timing?

SZ: Hopefully, Zoning review by the end of the year. The PD next year and the Environmental Report in the next month or so.

CM Question: Construction timing?

AB: We're hoping for June 2010.

CM Question: Do you have rear elevations? What will be visible?

CK: If you can't see the Baskin Robins building now, you won't see this building.

CM Question: The traffic is terrible. This is the last opportunity to widen the street.

CK: Street widening isn't in the scope of this project.

Date issued: 7 October, 2009

CM Question: Do you plan to salvage the other two older buildings?

CK: We will try.

CM Question: Any thoughts about a movie theater?

CK: No, there's not enough space.

CM Question: What does the back look like? Will retail be accessible from the rear?

CK: We're not certain yet (about access). There will be access from the rear to the plaza through the grand lobby. There is service access for the retail in the rear. We're not certain if the public will be able to access the retail from there – it's complicated with the service access.

CM Question: Does your plan address congestion? What about pedestrians?

CK: We don't control the street. We're making deeper sidewalks for public use.

CM Comment: I like your design. Design for people, not cars. Three stories are fine.

CM Comment: Kevin Swanson of the Willow Glen Business Association: We support the project. It's a nice gateway to the area. We're working with the team to come up with standards for the street. We support the proposed bus-stop modifications. It's the best solution I've seen to date.

CM Question: What about garbage pick up?

CK: Probably be in this zone (shows on drawing - near Willow Street entrance) or possibly concealed below the ramp.

CM Question: Is the rear wall 8-feet?

CK: We're doing 6-feet at the rear of the parking deck and 8-foot minimum at the ramp.

CM Comment: David O'Mara, Aqui Restaurant: Business has moved to Santana Row and other new centers. We need the economic vitality this project will bring. The day Peet's came in Aqui's business doubled. We need to make compromises. We need this project to help make this a community. At any time this would be a great thing for the community, in these economic times, it's heroic.

CM Comment: Regarding construction safety, please make sure access is restricted and sound contained.

CM Comment: I grew up in Saratoga. Resistance to change killed their economic vitality. It's great to hear about this project. I'm in support and hope it goes through. What can we do to help it go through?

SZ: Give us your input.

ME: It's important to keep hearing from you. We'll try to capture the flavor of the comments. Send us letters and emails. We'll make them part of the package. It really makes a difference.

Date issued: 7 October, 2009

SZ: Look for the flyer on the table up front and go on-line to do the survey.

CM Question: How many additional parking spaces will be provided?

SZ: 160 spaces

CM Question: How many are new?

SZ: 80

CM Question: Is it possible to go underground (with the parking)?

CK: It's possible but tends to be under-utilized.

CM Question: Can you give us a more firm timeline, or updates on the timeline of the project as it changes, progresses?

SZ: The Environmental Report will be on the website. The goal is to get rezoning done by the end of the year. Sjpermits.org has information on permits.

AB: Once we start, it's 16 months to completion. So it could open 2 years from now.

CM Comment: Michael Mulcahy: I own properties nearby. Alex wants to be sensitive to the community. People should be thankful that local property owners are here and involved. Real investment has been made here. We're trying to set a good example. This is a long-term investment in the community.

CM Comment: Willow Glen is lucky to have this quality commercial area. The sensitivity the developer has shown is great. I will stay here and shop here.

CM Question: Jeff of Willow Glen Frozen Yogurt Co: Where will construction start?

AB: We'll meet you with separately on the details.

CM Question: Will the PD Permit be done by the Planning Director or Commission?

SZ: The Director.

CM Question: Where will the large equipment be staged, parked?

CK: On-site.

Date issued: 7 October, 2009

Kahn Design Associates will rely on these notes as the approved record of matters discussed and conclusions reached during this meeting unless written notice to the contrary is received within five calendar days of the issue date of these meeting notes.

Zarnowitz, Sally

From: Lisa Poyser [lpoyser@pacbell.net]
Sent: Thursday, September 24, 2009 3:58 PM
To: Zarnowitz, Sally
Subject: RE: No. PDC09-016/PD09018

I'm all for progress, I've been in the San Jose and Saratoga area since 1953 and have seen plenty of it. It seems that the majority is going on in Willow Glen lately. Large, and I mean large, condo developments that will impact traffic around here and nothing has been done with the roads to upgrade them. Willow Glen does not need to look like downtown San Jose and have almost an acre under a roof especially on the corner of Lincoln and Willow. That corner is already jammed with traffic at the wrong time of day and when the bus decides to stop and have a break it is worse and there is a bus stop on opposite corner also.. Vehicles going in and out of the corner of Lincoln and Willow create quite a back up and its not much better trying to get out of the parking lot behind Aqui's on a Friday evening when all the traffic is moving along Willow Ave. One exit out of that large parking lot is not my idea of a good plan. Keep these things out of down town and have them build where there is room for the cars and people to get around. Down town needs to keep what's left of its small town charm and especially the yogurt store. Small guys are being moved out. Not good.

John Poyser

Zarnowitz, Sally

From: Sally Rae Larson [LarsonSally@fhda.edu]
Sent: Wednesday, September 23, 2009 11:54 AM
To: Zarnowitz, Sally
Subject: Lincoln/Willow proposed development

Hello Sally - I'm writing to say that our family has been very saddened to think that the gorgeous old trees will be taken out, the nice shady place where families gather to eat yogurt and have fun will be destroyed, and yet another building - 2 stories - will be added to the "landscape" in Willow Glen.

I am cognizant of an owner's right to develop his property, and I'm sure the zoning is appropriate to Mr. Byer's plan, but it's just a shame when there are so many empty spaces along the Avenue that this one oasis has to give way to more greed.

When you arrive "downtown" in Willow Glen, that very spot tells you that's IT - the glen, the trees, the slower pace of a small town. Too bad Mr. Byer can't see that it's great as it is. And surely the income isn't bad - it's always busy!

I realize nothing will stop "progress", but I wish the needs and wants of consumers fell into the mix somewhere - we WANT the yogurt shop and the shady trees - we choose that place to spend money. I guess Mr. Byer thinks more restaurants and retail will do better than the ones already in town.....?

As for parking, the only time it could be a problem is during "events" in W.G. and in general most residents can walk or bike to the Avenue! I am amazed that we 'need' a parking garage.

Thanks for listening. I'll be at the meeting on Thursday night.

Sincerely,

Sally Larson

Zarnowitz, Sally

From: Marshman, Barbara [BMarshman@mercurynews.com]
Sent: Friday, September 18, 2009 5:09 PM
To: Charles Kahn; Chris Rowe; barbaramarshman@hotmail.com; Zarnowitz, Sally
Subject: RE: Regarding our meeting on September 16th, 2009

Thanks for keeping us in the loop, Charles. And yes, Chris, Wil and I definitely agree privacy is paramount.

On the wall, one of our additional concerns is its appearance. Since the trees now along our back wall in the parking lot have not really thrived, they can't be expected to grow tall when their light is severely restricted. I believe we talked about vines, which would be great but can die, so some kind of design or material that's attractive on its own would be important. (Charles, I know you'll say Alex would take care of the vines, but we can't assume Alex will always own the property, even though I'm sure that's his intent now.)

I'm not sure we raised this concern in our original conversation, so I wanted to pass it on.

Best,
 Barbara

From: Charles Kahn [mailto:charlesk@kda-berkeley.com]
Sent: Friday, September 18, 2009 3:12 PM
To: Chris Rowe; Marshman, Barbara; barbaramarshman@hotmail.com; Zarnowitz, Sally
Subject: Re: Regarding our meeting on September 16th, 2009

Thanks Chris, for the clarification of your desires.

We will be submitting this revised (higher, 8-12' at the ramp, 6' at the parking deck) wall design with our PD set on Tuesday. I will forward your comments to the assigned senior planner, Sally Zarnowitz with the revised design. I have also discussed the design over the phone with your neighbor, Barbara Marshman, and she has indicated support of the higher wall as well, for purposes of privacy. We will be sending Barbara and Wil copies of the materials we provided you for their review and any further comment they may have.

Charles

On Thu, Sep 17, 2009 at 2:10 PM, Chris Rowe <crowe@gvakm.com> wrote:

Charles,

Thanks for taking the time on Tuesday to meet with me to discuss the development at Lincoln and Willow, specifically the sound wall.

Per our conversation and the sketches you provided to me, Ellen and I would like the highest wall with the less stair-step design as shown in one of your sketches. (still stair-step though but not as many steps as the other option). We feel it is very important to have this high wall to not only screen all automobiles including the UPS and FedEx delivery trucks, but also help reduce as much noise as possible.

And again we appreciate you taking the time to explain all of these things with us.

Thanks,

Chris

Zarnowitz, Sally

From: cecilychang@comcast.net
Sent: Friday, September 18, 2009 8:02 PM
To: Zarnowitz, Sally
Cc: cecilychang@comcast.net
Subject: PDC09-016/PD09-018 Proposed structure

Hello,

I am writing to express an opinion about the proposed 41,500 square foot building and attached parking structure at the corner of Lincoln and Willow.

I am against building such a large structure. We already have many vacant office and business spaces in Willow Glen, so I do not see the need for an even larger one building.

I do not want to encourage more traffic, and there will probably be cars wanting to turn into and come out of the parking structure, making it even more hazardous for cyclists and pedestrians. This intersection is really challenging for cyclists. We get clipped by cars wanting to turn right and we have to ride defensively when the buses and big trucks go by. The road is in bad condition with many longitudinal cracks. Bike tires can get caught in the cracks. The parking structure would encourage more traffic at this location. Small children ride their bikes on the sidewalk often against the traffic, so cars will have to be aware of cyclists coming from both directions. Sometimes, cars are so intent on beating the light that they do not stop for pedestrians nor do they see cyclists.

Also, this building will likely be several stories and will look out of place with the rest of the buildings in Willow Glen.

Willow Glen needs a good grocery store and not more specialty shops or boutiques. We do not need another hair salon, nail parlor or liquor store. It is too bad we lost the Willow Glen bookstore.

Sincerely,

Cecily Chang



**Willow Glen
Business Association**

**2009-2010
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Linda Waltrip
Park Place Vintage

Debby McCarty
Community Member

Executive Director
Norma Ruiz

September 18, 2009

Ms. Sally Zarnowitz
200 East Santa Clara Street
Tower, 3rd Floor
San José, CA 95113-1905

Re: PDC-09-016

Dear Ms. Zarnowitz:

The Willow Glen Business Association is pleased to give our support to the proposed development at the southeast corner of Willow Avenue and Lincoln Street. This project supports our vision and strategic plan of creating "an economically strong, pedestrian-friendly business district of shops, services, dining and cultural uses serving The Greater San Jose Area" ... and "making downtown Willow Glen more pedestrian and customer friendly".

During this economic downturn it is exciting for our business district to have such a substantial economic investment, which will not only produce a quality project, but add jobs to our local community.

We believe this project will be an attractive and welcome addition to economic, cultural and architectural fabric of our business district.

Other factors supporting our endorsement of the project are:

- The scale and mass of the building is appropriate to our unique business district and works with the branding of our district – Personal Friendly Different.
- The mix of offices, retail and restaurants will attract customers to the business district which will benefit all businesses in our district.
- The proposed public amenities (such as the large plaza and streetscape improvements) will add to the visual richness of district and a possible venue for cultural activities. Mr. Byer has been a strong supporter of our events by allowing use of his property.
- The design incorporates the Willow Street Pizza business and the historic building as well as the Willow Glen Frozen Yogurt Co. which they plan to keep open until their new space is ready for occupancy.
- The parking deck in the rear of the complex will reduce competition for on-street parking.

We encourage the City to cooperate with the developer's plans to move the bus stop at the corner to a more mid-block location in order to improve pedestrian and automotive safety and create a more pedestrian-friendly environment.

In the current economic environment, with many retailers shuttering businesses, we feel fortunate to have a new economic center proposed for the neighborhood. We know that for a business district to thrive, it must be allowed to grow and change in response to the community's needs. Based on the developer's previous successful projects in the area (1261 and 1140 Lincoln Avenue), we feel strongly that this project will help to revitalize the business district, thereby serving the greater needs of the Willow Glen community.

Sincerely,

A handwritten signature in cursive script that reads "Kevin C Swanson". The signature is written in black ink and is positioned to the right of the typed name.

Kevin Swanson
President

Blewett Avenue Neighborhood Association

1300 Blewett Avenue San Jose, CA 95125

May 27, 2009



Ms. Sally Zarnowitz
Planning Department.
City of San Jose
200 East Santa Clara Street
San Jose, CA

Dear Ms. Zarnowitz:

The Blewett Avenue Neighborhood Association (BANA) thanks the Planning Department for this opportunity to submit comments on PDC 09-018, 1104 Lincoln Avenue. BANA commends Paja Investments for its commitment to the Willow Glen Business District and its response to the City of San Jose's comments on its preliminary plan. Further, BANA commends the inclusion of a plaza and the generally smaller scale of this submitted design. This project represents a major change to Lincoln Avenue and will set the tone for future redevelopment. Careful attention to a well-designed interface with single family homes will help future applicants with their designs.

The Blewett Avenue Neighborhood Association (BANA) represents the residents and owners of Blewett Avenue, a group of about 65 historic homes. In the 1930s, Blewett Avenue was created by the action of the City of Willow Glen in order to support the business district with additional nearby residents. Continuing these historic roots, BANA is committed to a cooperative and collaborative relationship with the Willow Glen Business Association in order to create a pedestrian friendly environment that celebrates the historic neighborhood and allows both residents and businesses to thrive.

Regarding the proposed development, here are BANA's concerns:

1. Traffic. The traffic along westbound Willow Street regularly backs up to Blewett Avenue. This suggests that the Willow Street exit must be right turn only. BANA is concerned that there will be an increase in drivers who will use Blewett Avenue for a quick U-turn. Residents near the Blewett Avenue driveway of the "31 Flavors/Gagliardi Office Building" see frequent near misses. We believe there will be fewer near misses if vehicles entering Blewett are required to enter more slowly. Blewett is already approved for a bulb-out at the corner of Willow Street. BANA believes an appropriate mitigation for increased traffic on Blewett is construction of this bulb-out. It will reduce speeds at the entry to the street. BANA is prepared to adopt and maintain the bulb-out.

Q1. Will traffic mitigation for this project include a Blewett bulbout?

As frequent pedestrians, the entrances along both Willow and Lincoln concern us. Currently, when drivers attempt to enter from southbound Lincoln, they accelerate to “beat” oncoming traffic. This is dangerous for pedestrians. We expect a similar problem will exist for westbound cars accessing the project from Willow Street.

Q2. Will it be possible to forbid left-turn access from these directions?

Q3. Will the entrances be made narrow so that the pedestrians’ danger area is as small as possible?

2. Cultural Resources. We applaud the applicant’s intention to include the historic brick building. We are concerned that the second story design and massing overwhelms the brick building. The historic building does not have high visibility and presence. Also, very little of the original brickwork is exposed because of the design of the outdoor heating area. Applicant’s renderings suggested that the adjacent building on the west side would use materials that were very similar to the brick work. This choice of materials “swallows” the historic building. We believe that the historic building is an important component of the historic story of Willow Glen and its business district.

Q4. Will design modifications be encouraged by the City?

In addition, the corner of Willow and Lincoln was once the home of Campbell T. Settle, mayor of San Jose and principal land owner of Willow Glen. His home place covered the parking lot of the liquor store and the park behind the current Yogurt Shop. We hope that the applicant would be willing to cooperate with the Willow Glen Business Association’s Historic Sign project and place a plaque honoring Mayor Settle.

Q5. If a deposit of artifacts from the 1860s-1890s are found (possibly from the outhouse) during construction, will an effort be made to collect and examine them?

Many of our members appreciate and value the historic home on Willow Street.

Q6. Will efforts be made to relocate or recycle this structure?

3. Transit. The Willow Glen Neighborhood Business District is a pedestrian friendly environment. Architect Charles Kahn told the community in December 2008 that this design utilized transit-oriented design features. BANA notes that the submitted plans do not show the bus stop. Page A1.0 suggest that the bus stop will be demolished. The Willow and Lincoln intersection is designated a “Local Core” by VTA and is a major transfer point between Routes 25 and 64; both are key VTA. BANA is opposed to moving this bus stop away from the intersection towards residences, burdening both residents and transit riders trying to transfer at Lincoln Avenue. BANA believes transit-oriented development should accommodate transit stops in its design.

BANA notes that the current bus stop location is immediately adjacent to a planned door opening in retail unit 101. Given the heavy use of this bus stop, this will cause a user conflict between pedestrians, bus users, and shoppers at unit 101. This suggests the retail unit should be redesigned or the bus stop moved closer to Lincoln Avenue.

Q7. Although we are opposed to moving the transit stop, if the bus stop is moved, where will it go, and how will its impacts be mitigated?

4. Size of Project. Aesthetics.

We note that the three-story building does not seem to conform to the Willow Glen Business District Revitalization Design Guidelines. It is not clear to us that Lincoln Avenue has adequate width to absorb a project of this scale and maintain a pedestrian friendly feel. To help community members understand the size and scale of new structures, other jurisdictions require PVC scaffolding to be built prior to the community meeting. This seems highly appropriate for such a major change to the principal gateway of this historic shopping district.

Q8. Prior to community meetings, will the applicant be asked to construct “PVC” structures to show the overall massing of the project?

However, we do appreciate that this revision shows that the third story of the project is situated away from the single family homes of Blewett Avenue. We hope that privacy of single family homes will not be impacted by the third floor.

Q9. Will the applicant be providing a view line analysis for residents of the single family homes in order to allay their fears?

Also, the massing of the project will be highly visible on Blewett Avenue to residents, pedestrians and bicyclists. Blewett is used heavily by local walkers from the commercial buildings and as the bicycle route that is preferred over Lincoln Avenue. The plans show only a few small trees in the parking lot, which does not seem to meet San Jose’s LEED standards. From BANA’s perspective, softening of the view is the most important priority. Landscape screening along the eastern edge would serve to soften the pedestrian view and provide applicant with the greatest freedom between using trees or trellis on the rest of the lot to meet LEED standards.

Q10. Would it be possible to provide a landscape screening along the eastern wall of the parking structure, perhaps with trees or bamboo?

5. Parking. We note that this project is using the reduced parking ratios associated with the new Commercial Pedestrian (CP) Zoning. At the time of the implementation, Willow Glen Business Association and Planning committed to converting the city parking located at the Bank of America from short-term parking to a length of time that is compatible with employee shifts.

- Q11. Will this parking hour conversion be completed prior to this project's construction?
- Q12. In this project, will the second floor parking structure be restricted to office users?
- Q13. Where will employees of retail and restaurants park?
- Q14. Will employees be asked to park at Bank of America public lot?
- Q15. Will a parking analysis be conducted?
- Q16. Will an estimate be made for how many spaces on Blewett will be needed to support employee parking?
- Q17. If the Bank of America lot is not converted to extended parking, will Planning support a permit parking area for the northern half of Blewett Avenue?

6. Streetscape. As a pedestrian friendly development, the sidewalks surrounding the development should be wider than the minimum. We recognize that the historic building limits the width of sidewalk at that portion of the development, but believe that the rest of the development should have wider sidewalks that accommodate full size trees at regular intervals. It seems that there are fewer shade trees along Willow, as well as the area of user conflict with the bus stop.

- Q18. Will there be more trees than shown on the current plan?

7. Property Line Sound Wall. Adjacent residents ask,

- Q19. Will the existing sound wall be retained?
- Q20. Will the height reach 8 feet after grading?
- Q21. Will repairs be made to existing sound wall as part of this permit?

8. Ramp and ramp walls. The current design shows a ramp that starts at 8% grade, followed by 16%, ending at 8%.(See A1.2). The ramp walls show a "stair-stepped" design. Plans show a person at the top of the ramp, giving guidance to the planned height. There are many questions.

a. Grade. One of BANA's members is a professional truck driver. He notes that the 16% grade is extremely steep for trucks and cars.

- Q22. Will trucks be forbidden from the second floor?

Further, as a professional driver, he notes passenger vehicles will have a very difficult time getting restarted if they must stop on the 16% grade. While 4WD vehicles will easily deal with this, drivers with clutches may find it necessary to roll down to the bottom of the ramp in order to restart. He wonders what might happen if there is a traffic jam at the top as people enter and exit parking spaces. On the whole, Willow Glen residents do not have the hill driving skills that San Franciscans and mountain people possess; we are flat-landers.

- Q23. Will there be signs at the bottom warning drivers?
- Q24. Will there be a light limiting only one driver at a time? If so, how will the light be shielded so that adjacent neighbors are not impacted?

Q25. Will this steep grade discourage use of the second floor parking lot and encourage vehicles to use Blewett for parking?

b. Ramp Noise. Residents are concerned about the noise created by acceleration at the start of the 16% grade and the braking sounds through this steep descent. They express concerns about the sounds caused by cars crossing grates needed for stormwater, traffic, or skateboarder control. They wonder,

Q26. Will the materials in the ramp wall be designed to absorb sound?

c. Privacy and Safety. Residents do not wish to view vehicles climbing the ramp.

Q27. Will the “stair step” design be configured so that the vertical distance from the road surface to the top of every part of the ramp wall will be taller than passenger vans?

Q28. Will the walls be taller at all portions so that pedestrians or skateboarders cannot view or throw things into backyards?

d. Safety.

Q29. What measures will be taken to prevent skateboarders?

Q30. Will the tops of the ramp walls be configured to prevent Extreme Sport enthusiasts, i.e. those who use structures for jumps, hops, and skateboarding?

Q31. What design features will the applicant include to prevent loitering after hours?

Q32. Will the ten foot setback between the ramp wall and the sound wall be fenced and gated?

Q33. Will the second floor lot be gated after hours?

e. Aesthetics. The parking structure is expected to outlast adjacent properties’ trees and landscaping.

Q34. What design features will be incorporated into the ramp wall itself so that it is architecturally more interesting and not a bare, flat wall?

Q35. Will a shade loving vine or other landscaping be planted to soften the wall’s massing?

The current plan shows that existing pines in the ten foot setback are to be retained. However, residents are concerned about their long-term survival in the ten-foot “dead space” between the sound wall and the ramp wall. Residents wonder:

Q36. Are there shade tolerant trees that will reach heights of 15 or 20 feet or more that may be appropriate?

Q37 Will the plants receive watering?

The two northernmost adjacent homes have many trees along their property lines. They are very concerned that construction not impact their trees and the branches which extend slightly into the subject property.

Q38. Will construction permits be issued with the requirement that the branches will not be

trimmed and will be protected during construction?

f. Safety. Current plans call for a transformer box adjacent to the current box at the corner of the northernmost home. Access is planned via the 10 foot setback. The property owner wonders whether access could be provided via a door in the ramp wall, through the storeroom underneath the ramp.

Q39. Would this direct access be more conducive to the health of the shady plants in the setback, minimize safety concerns due to workers carrying tools along a landscaped area?

g. Stormwater. Residents express concern about the handling of storm water that may end up in the 10 foot setback.

Q40. How will this water be diverted away from their property and away from the setback so that plants will be more likely to survive?

h. Trash. The current plans show southern trash and recycling enclosures adjacent to the building. Residents encourage trash bins to remain as far as possible from single family homes.

Q41. Will the final plans keep trash cans away from residences?

8. Air pollution. Residents expressed concerns about the pollution that likely will collect in the underground parking area. Prevailing winds will blow the pollution to their homes.

Q42. Will mechanical vents be installed?

Q43. Will they have some sort of scrubbing device?

Q44. What noise level will mechanical scrubbers contribute to the ambient noise?

9. Construction Hours, Dust, Noise. Residents expressed concerns about construction.

Q45. Does this project require any pile driving?

Q46. Will construction hours be limited?

Residents are aware that applicants may ask for extended construction hours as a permit adjustment after the PD permit is issued.

Q47. Will they be notified of an application?

Q48. Under what conditions will construction hours be extended?

Residents are opposed to weekend construction hours. BANA encourages the applicant to schedule for rain delays.

Q49. Will the applicant be required to schedule for rain delays?

Q50. Will construction equipment have state-of-the-art mufflers and noise deafening shields?

10. Vibration Damage. When the applicant constructed the “Jamba Juice” building, adjacent residents were informed of a claim system for vibration damage to their property caused by

grading and heavy equipment.

Q51. Will the applicant use a similar system for this project?

Q52. How will claims be handled?

Q53. To what is the city involved with requiring these mitigations and compensations?

11. Hazardous Materials. Older structures with lead based paint and other hazardous materials will be demolished. The corner of Willow and Lincoln under the yogurt shop was once a gas station. Given the daily prevailing winds, the single family homes are downwind from the demolition.

Q54. How will the demolition be staged so that hazardous materials are not released into the wind?

Q55. Will soil samples be taken at the gas station site?

Q56. How will contamination be handled?

Q57. Where will trailer trucks for demolished materials be loaded?

Q58. Will this be away from the single family homes?

12. Construction Traffic and Parking. Residents are concerned about the impact of construction workers and equipment. Willow Street is very damaged. Heavy construction equipment and trailers will further damage the street. Blewett Avenue is closed to vehicles over 5 tons.

Q59. Will heavy equipment and trucks be restricted from Blewett?

Q60. Will heavy equipment be required to use Lincoln Avenue to exit the neighborhood?

Q61. Will heavy trucks and construction equipment be forbidden from Willow Street?

Q62. Will construction workers be required to use the underutilized extra City parking on the Bank of American property?

13. Construction Staging. BANA is concerned about the staging of construction materials and equipment.

Q63 During each phase of construction, where will the construction management office be located?

Q64. Where will heavy equipment be stored?

BANA recognizes that the city has a process for applicants to apply for temporary closures of streets. Willow Street is extremely narrow and carries a heavy load of traffic. To the maximum extent possible, BANA respectfully requests that Willow Street not be blocked and that BANA residents be notified via our Google Group whenever it is blocked.

Q65. Can the City require applicant to notify BANA of Willow Street disruptions?

Further, we respectfully request that the applicant provide notification to Blewett's Google Group whenever another phase of construction is anticipated to start. Many of our members

have small children who would enjoy watching grading, cranes, and similar activities.

Q66. Will the City request applicant to notify BANA of interesting construction activities?

14. Deliveries and Hours. Currently, many businesses on Lincoln Avenue and Willow Street receive deliveries on street and create traffic jams. BANA encourages a delivery location on site and not on Lincoln or Willow.

Q67. Will delivery hours be restricted to after 6:30 am?

15. Environmental Review.

Q68 Will there be an EIR for this project or an IS?

Thank-you for this opportunity to express our concerns and look forward to working with the applicant and you as this project moves forward. Please contact me or board member Jean Dresden if you have any questions.

Sincerely,



Bill Cardoza
President
bill_cardoza@yahoo.com

cc. Jean Dresden
jeanann2@aol.com
298-0275

Zarnowitz, Sally Notthoff

From: todd Weber [toddnll@mac.com]
Sent: Tuesday, May 19, 2009 9:04 PM
To: Zarnowitz, Sally Notthoff
Subject: Lincoln and Willow Street Development Proposal

Ms. Zarnowitz,

I understand that you have been advised that the community here in Willow Glen universally liked the proposed development presented to the community and neighbors.

Simply, that was not and is not the case. My wife and I both attended the public session hosted by Councilman Oliverio. Many attendees had explicit concerns and requests to Mr. Byer and his architects - all of which were claimed to be taken down to be considered but few if any apparently were forwarded to you.

My wife and I own our home and live on Blewett Avenue across the street from the block-end where the proposed development would occur. We literally will see the result of said development every day from our living room window - suffice it to say we hope you will be able to apply appropriate assessment of the proposal before submitting your conclusions - though we also completely appreciate that in today's budget-constrained economy you are probably no less work-overloaded than we are.

So we just hope you'll be able to assist Mr. Byer in appreciating the rules and laws his final development will be required to comply to.

In any case, below is the email my wife sent to Councilman Oliverio's office in response to the public session. Please feel free to give us a call if you feel we can assist your efforts.

Best regards,

Todd Weber & Linda Lee Kerr
1134 Blewett Ave.
San Jose, CA 95125
(408) 292-2597

----- Forwarded Message

From: linda lee kerr <lindaleekerr@pacbell.net>
Date: Mon, 23 Feb 2009 13:41:50 -0700
To: <Pierluigi.Oliverio@sanjoseca.gov>
Conversation: 1130 Willow project
Subject: 1130 Willow project

I am slow to respond, I know, but feel compelled to do so just the same...

I was one of the 100 in attendance at the December 17th meeting for the above mentioned project. Following are my thoughts and impressions about the proposed project presented at that time:

1. Too big for the location
2. Too bland for historic downtown Willow Glen
3. A parking structure, even a two-story one, sends the wrong message. We don't need more cars driving downtown. Lincoln Avenue, Willow Street, and Minnesota already back up enough twice a day. We need more people traveling by bus, VTA, etc. Shouldn't any development be more "green" rather than less? More cars=less "green".

I feel such a large project as proposed by Mr. Byer would be so out of character to the rest of Lincoln Ave. and surrounding neighborhood that it would be seen as a "blight" rather than an improvement. Willow Glen is known for "character and charm." People traveling to our little part of San Jose do so expecting to find a "unique" place.

In my opinion, Mr. Byer's project does not reflect the best that Willow Glen has to offer.

Regards,
Linda Kerr

----- End of Forwarded Message

Zarnowitz, Sally

From: todd A. Weber [toddnlil@pacbell.net]
Sent: Monday, May 18, 2009 9:32 PM
To: jeanann2@aol.com
Cc: todd Weber
Subject: FW: 1130 Willow project

Jean Ann - please forward to the analyst for us. Thx - todd & linda

----- Forwarded Message

From: linda lee kerr <lindaleekerr@pacbell.net>
Date: Mon, 18 May 2009 19:56:39 -0700
To: todd Weber <toddnLiL@mac.com>
Subject: FW: 1130 Willow project

----- Forwarded Message

From: linda lee kerr <lindaleekerr@pacbell.net>
Date: Mon, 23 Feb 2009 13:41:50 -0700
To: <Pierluigi.Oliverio@sanjoseca.gov>
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In my opinion, Mr. Byer's project does not reflect the best that Willow Glen has to offer.

Regards,
Linda Kerr

----- End of Forwarded Message

----- End of Forwarded Message

1104 LINCOLN AVENUE

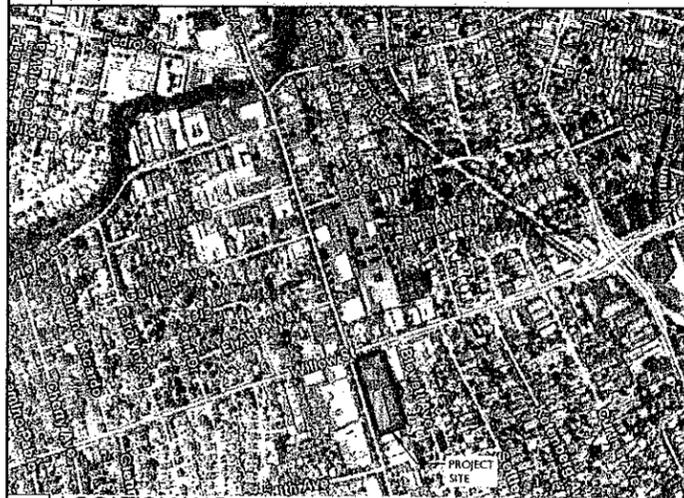
WILLOW ST. & LINCOLN AVE.

SAN JOSE, CA

PLANNED DEVELOPMENT ZONING
JANUARY 4, 2010



6 CONCEPTUAL PERSPECTIVE SKETCH FROM INTERSECTION
NOT TO SCALE



8 LOCATION MAP
SCALE: 1"=500'

7 PRELIMINARY CONSTRUCTION SCHEDULE

PHASE	DETAILS	DURATION	START DATE	END DATE
PHASE 1	DEMOLITION & STAGING	2 MO.	February-2010	April-2010
PHASE 2	PRECAST PARKING DECK	2 MO.	April-2010	June-2010
PHASE 3	WARM SHELL STRUCTURE	9 MO.	June-2010	March-2011
PHASE 4	CITY SIDEWALK	2 MO.	January-2011	March-2011
PHASE 5	PLAZA FOUNTAIN AREA	6 MO.	December-2010	July-2011

7 CONSTRUCTION SCHEDULE

APN: 429-06-070, -069, -068, -067, -061, -002
 ZONE: CP (PD) COMMERCIAL PEDESTRIAN NEIGHBORHOOD BUSINESS DISTRICT
 USE: M, B, A
 OCCUPANCY: RETAIL FOOD SERVICE, OFFICE
 CONSTRUCTION TYPE: TYPE V, A
 FIRE SUPPRESSION: SPRINKLER SYSTEM
 PERMITS: PLANNED DEVELOPMENT ZONING #PDC09-016
 PLANNED DEVELOPMENT #PD09-018
 DEFERRED PERMITS: PUBLIC RIGHT OF WAY IMPROVEMENTS UNDER SEPARATE PERMIT

FLOOR AREA ANALYSIS

FLOOR AREA	USE	EXISTING (SF)	NEW (SF)	TOTAL (SF)	FLOOR AREA (85% REDUCTION)	USE	SPACES
(E) 1 STORY BUILDING	FOOD SRV/ RETAIL	15,023		15,023	12,770	400	32
(N) GROUND FLOOR	FOOD SRV/ RETAIL		14,373	14,373	12,217	400	31
(N)+(E) WILLOW ST. PIZZA	FOOD SRV.	1,875	1,010	2,885	2,452	400	6
(N) 2ND FLOOR	OFFICE		17,590	17,590	14,952	250	60
(N) 3RD FLOOR	OFFICE		8,530	8,530	7,251	250	29
TOTAL		16,898	41,503	58,401	49,641		158

PARKING ALLOWANCES

(N) GROUND LEVEL PARKING	37,600 SF	GROUND FLOOR SPACES	100
(N) PARKING PODIUM PARKING DECK W/ RAMP	23,158 SF	PARKING DECK SPACES	58
TOTAL	60,758 SF	LOADING SPACES	2

LANDSCAPE ANALYSIS

(E) ENTIRE SITE AREA	(2.077 ACRES)	90,476 SF
(N) BUILDING (GROSS SQ.FT.)	(65% SITE COVERAGE)	58,401 SF
(N) AREAS OF LANDSCAPING	(5% OF SITE COVERAGE)	4,281 SF

5 PROJECT INFORMATION

4 CODE SUMMARY

ALL WORK SHALL MEET OR EXCEED ALL CURRENT APPLICABLE BUILDING, ELECTRICAL, PLUMBING, MECHANICAL, SAFETY, ZONING CODES, AND ORDINANCES, INCLUDING BUT NOT LIMITED TO:
 2007 CALIFORNIA BUILDING CODE BASED ON THE 2006 EDITION OF THE IBC
 2007 CALIFORNIA PLUMBING CODE, BASED ON 2006 UPC
 2007 CALIFORNIA MECHANICAL CODE, BASED ON 2006 UMC
 2007 CALIFORNIA ELECTRICAL CODE BASED ON 2005 NEC
 2007 CALIFORNIA EXISTING BUILDING CODE
 2006 INTERNATIONAL EXISTING BUILDING CODE
 2005 CALIFORNIA BUILDING ENERGY EFFICIENCY STANDARDS

GENERAL DESIGN CRITERIA ARE AS FOLLOWS:

- SAN JOSE IS IN SEISMIC DESIGN CATEGORY D OR E (OR F FOR OCCUPANCY CATEGORY IV WHERE 512-075)
- BASIC WIND SPEED IS 85 MPH.
- SOIL BEARING PRESSURE IS BASED ON 1,500 POUNDS PER SQUARE FOOT, UNLESS JUSTIFIED BY A SOILS REPORT
- THE CLIMATE ZONE IS ZONE 4.
- RAINFALL DESIGN IS BASED ON TWO INCHES PER HOUR.

OWNER:
PAJA INVESTMENTS
66 POTRERO AVENUE
SAN FRANCISCO, CA 94103
TEL (415) 626-2937
FAX (415) 626-2767
ALEX BYER

ARCHITECT:
KAHN DESIGN ASSOCIATES
1810 SIXTH STREET
BERKELEY, CA 94710
TEL (510) 841-3555
FAX (510) 841-1225
CHARLES KAHN
charlesk@kda-berkeley.com
TODD POLISKIN
toddp@kda-berkeley.com

ENVIRONMENTAL ASSESSMENT CONSULTANT:
DAVID J. POWERS & ASSOCIATES
1885 THE ALAMEDA, SUITE 204
SAN JOSE, CA 95126
TEL (408) 248-3500
TANIA COTTLE
tcottle@davidpowers.com

CIVIL ENGINEER:
ALLIED ENGINEERING CO.
3170 WILLIAMS ROAD
SAN JOSE, CA 95117
TEL (408) 241-1960
FAX (408) 241-3047
LESTER IKEGAMI
allied@alliedengr.com

ARBORIST:
SBCA TREE CONSULTING
1534 ROSE ST.
CROCKETT, CA 94525
TEL (510) 787-3075
FAX (510) 787-3065
STEVE BATCHELDER
steve@sbcacree.com

HISTORICAL CONSULTANT:
ARCHAEOLOGICAL RESOURCE MANAGEMENT
496 NORTH 5TH STREET
SAN JOSE, CA 95112
TEL (408) 295-1373
FAX (408) 286-2040
ROBERT R. CARTIER
rmcartier@netcape.net

DESIGN COMPATIBILITY CONSULTANT:
MBA ARCHITECTS
MARVIN BAMBURG ASSOCIATES, INC.
1176 LINCOLN AVE.
SAN JOSE, CA 95125
TEL (408) 297-0288
FAX (408) 297-0384
MARVIN BAMBURG
marvin@mbs-architects.net

LIGHTING CONSULTANT:
ALICE PRUSSIN LIGHTING DESIGN
931 PARDEE STREET
BERKELEY, CA 94710
TEL (510) 525-3670
FAX (510) 525-3672
ALICE PRUSSIN
alice@illumina.com
PHAN N. DUNG
phan@illumina.com

3 PROJECT TEAM

PROJECT DESCRIPTION:

THIS PLANNED DEVELOPMENT CONSISTS OF A GROUND FLOOR RETAIL BUILDING WITH ONE TO TWO OFFICE FLOORS ABOVE AND AN ATTACHED PODIUM PARKING STRUCTURE. IT IS LOCATED AT THE SOUTHWEST CORNER OF WILLOW STREET AND LINCOLN AVENUE IN THE WILLOW GLEN NEIGHBORHOOD DISTRICT OF SAN JOSE, CALIFORNIA.

THE PROJECT GOALS ARE TO INCLUDE THE EXISTING WILLOW STREET PIZZA BUILDING, 1140 LINCOLN AVENUE, AND ADJACENT LOTS INTO A COHERENT, WELL DESIGNED, CONTEXTUALLY APPROPRIATE DEVELOPMENT FOR THIS COMMERCIAL ZONE TO IMPROVE SERVICES FOR THE NEIGHBORHOOD WHILE ENHANCING THE WILLOW GLEN BUSINESS DISTRICT. THE PROJECT WILL INCLUDE FACADE IMPROVEMENTS TO THE EXISTING STRUCTURE AT 1140 LINCOLN AVENUE AND DEVELOPMENT OF OUTDOOR AREAS FOR PUBLIC USE IN THE FORM OF A CORNER PLAZA, OUTDOOR RESTAURANT SEATING, AND IMPROVED PEDESTRIAN PATHWAYS.

2 SCOPE OF WORK

ARCHITECTURAL

- A0.0 COVER SHEET
- A0.1 CONCEPTUAL PERSPECTIVE RENDERINGS
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NOTE: LANDSCAPE & ARCHITECTURAL LIGHTING DRAWINGS ARE FOR REFERENCE ONLY. SEE PROJECT INFO S/A0.0 FOR DEFERRED PERMITS.

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AND AT DIAMETER OR ROUND	HORIZ. HORIZONTAL	HARDWARE HORIZONTAL	INT. INSUL.	INTERIOR INSULATION
ABV. ABOVE	AIR COND. AIR CONDITIONING	ACCIDENTAL CEILING TILE ADJACENT OR ADJUSTABLE	K.E.C. KITCHEN EQUIPMENT CONTRACTOR	MAX. MAXIMUM
A.C. ALTERNATE	ARCH. ARCHITECTURAL	B.L.D.G. BUILDING	B.L.K.G. BLOCKING	B.O. BOARD
B.M. BEAM	B.O.T. BOTTOM	C.A.B. CABINET	C.G. CORNER GUARD	C.J. CONTRA JOINT
C.L. CENTER LINE	C.L.R. CLEAR	C.L.G. CEILING	C.L.O. CLOSURE	C.O. CLEANOUT OR CASED OPENING
C.M.U. CONCRETE MASONRY UNIT	C.N.C. CONCRETE	C.N.T. CONSTRUCTION CONTINUOUS	C.O.L. COLUMN	C.P.T. CARPET
C.W. COLD WATER	D. DAYER	D.T.L. DETAIL	D.I.A.M. DIAMETER	D.I.M. DIMENSION
D.R. DOOR	D.W.G. DRAWING	D.F. DOUGLAS FIR OR DRINKING FOUNTAIN DISPENSER	(E) EXIST. EXISTING	E.A. EACH
E.L. ELEV. ELEVATION	E.L.C. ELECTRICAL	E.F. ELECTRICAL PANELBOARD	E.Q. EQUAL	E.Q.U.P. EQUIPMENT
F.B.O. FURNISHED BY OTHERS	F.D. FLOOR DRAIN	F.F. FINISHED FLOOR	F.T. FEET	F.L. FLOOR
F.L.C. FLOOR JOIST	F.O.F. FACE OF FINISH	F.O.S. FACE OF STUD	F.N. FINISHED	F.S.E.C. FOOD SERVICE EQUIP. CONTRACTOR
F.R.P. FIBERGLASS REINFORCED PANEL	F.G. FACE OF	G.A. GAUGE	G.C. GENERAL CONTRACTOR	G.F.R.C. GLASS FIBER REINFORCED CONCRETE
G.L. GLAZING, GLASS	G.S.M. GALVANIZED SHEET METAL	G.Y.P.S.U.M. GYPSUM WALL BOARD	HORIZ. HORIZONTAL	INT. INSUL.
HORIZ. HORIZONTAL	INT. INSUL.	K.E.C. KITCHEN EQUIPMENT CONTRACTOR	MAX. MAXIMUM	MECH. MECHANICAL
MOUNTED MATERIAL METAL	N.W. NOT IN CONTRACT	N.T.S. NOT TO SCALE	O.V. OVER	O.N. CENTER
O.W.S. OWNER SUPPLIED, CONTRACTOR INSTALLED	P.B.O. PROVIDED BY OWNER	P.L. PLATE	P.L.A.M. PLASTIC LAMINATE	P.L.Y.W.D. PLYWOOD
P.T.D. PAINTED	R.E.F. REFRIGERATOR	R.E.G. REGISTER	R.O. ROOF DRAIN	R.O. ROUGH OPENING
S.E.C. SECTION	S.H.T. SHEET	S.P.R. SPRINKLER	S.K.Y.L.I.G.H.T. SKYLIGHT	S.Q. SQUARE
S.S. STAINLESS STEEL	S.S.D. SEE STRUCTURAL DRAWINGS	T. TREAD	T.A.G. TONGUE AND GROOVE	T.B.D. TO BE DETERMINED
T.E.L. TELEPHONE	T.E.M.P. TEMPERED	T.H.R.U. THROUGH	T.Y.P. TYPICAL	T.V. TELEVISION
U.L. UNDERWRITERS LABORATORY	U.O.A. UNLESS OTHERWISE NOTED	V.E.R.T. VERTICAL	V.I.F. VERIFY IN FIELD	V.G. VERTICAL GRAIN
W. WASHER	W.I.T.H. WITH	W.C. WATER CLOSET	W.D. WOOD	W.I.T.H.I.N. WITHIN
W.I.T.H. OUT. WATERPROOF	W.A.I.N.S.C.O.T. WATER HEATER	W.A.L. WALL		

10 ABBREVIATIONS

KEY NOTE: SEE LEGEND ON DRAWINGS FOR EXPLANATION OF EACH NOTE

DATUM REFERENCE: ELEVATION, WORK OR CONTROL

DOOR SYMBOL: DOOR MARK OR SEQUENCE NUMBER

WINDOW SYMBOL: WINDOW MARK OR SEQUENCE NUMBER

WALL TYPE REFERENCE

MATERIAL/FINISH NOTE REFERENCE

PLUMBING SYMBOL/REFERENCE

APPLIANCE SYMBOL/REFERENCE

DIMENSIONS:

FACE OF FINISH

CENTERLINE

CLR./ MINIMUM CLR.

ALIGN FINISH SURFACES

CHANGE IN FINISH FLOOR MATERIALS

STEP DOWN (DIMENSION INDICATED)

SECTION REFERENCE

ELEVATION REFERENCE

COMBINED INTERIOR ELEVATION MARKER

DETAIL/SECTION REFERENCE

ENLARGED PLAN/ DETAIL REFERENCE

ENLARGED ELEVATION

PROJECT NORTH ACTUAL NORTH

NORTH ARROW

REVISION REFERENCE

9 GRAPHIC SYMBOLS

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kda

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COVER SHEET

SCALE: AS NOTED

DRAWN BY: SAE

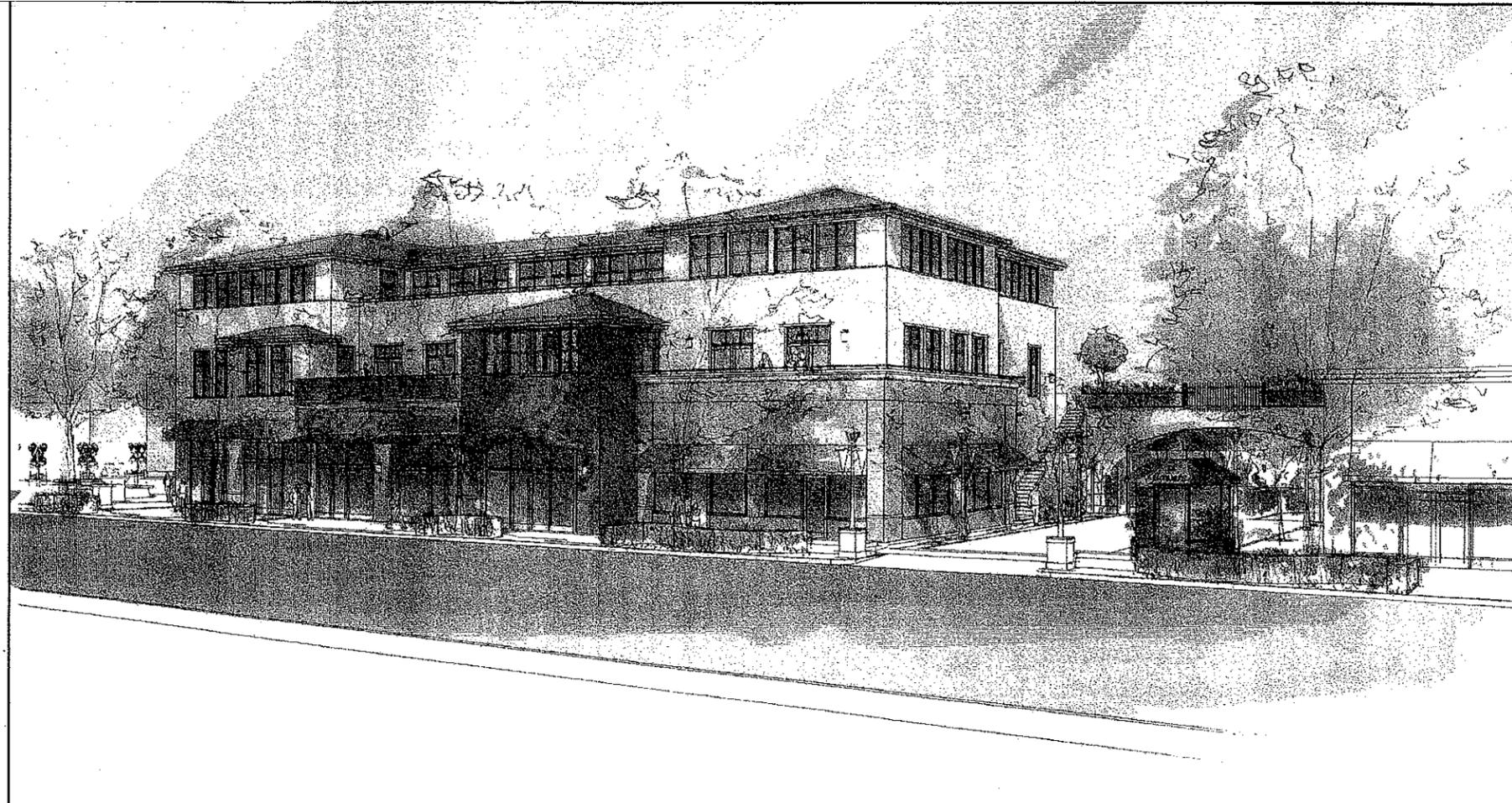
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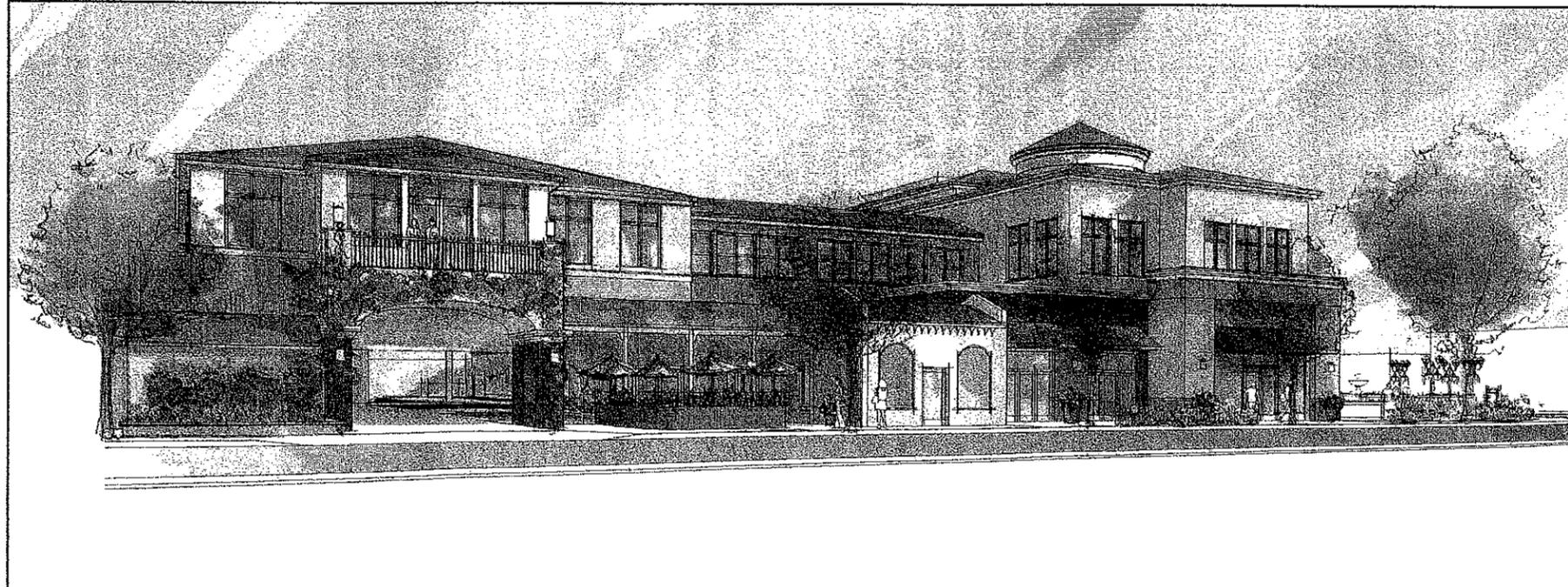
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2 CONCEPTUAL PERSPECTIVE RENDERING FROM LINCOLN AVENUE
NOT TO SCALE

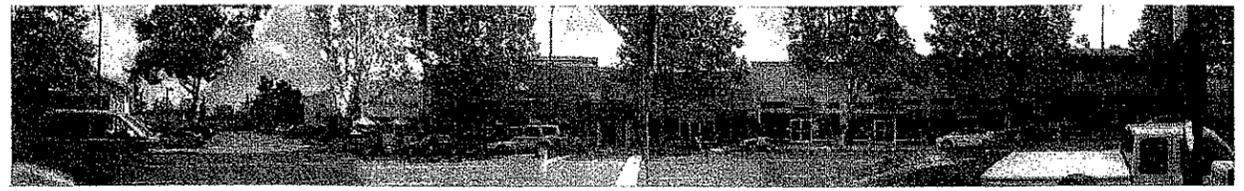
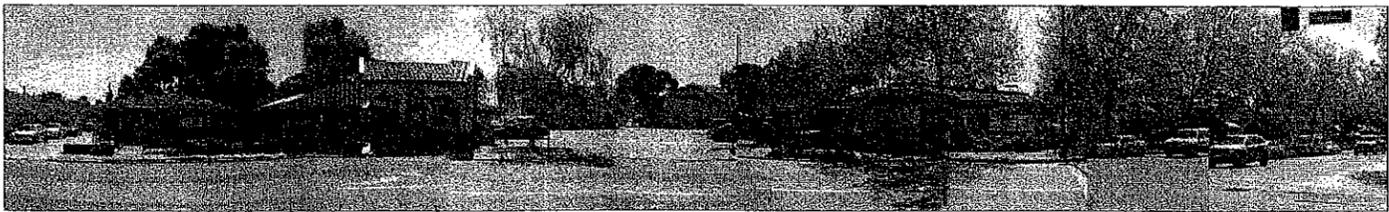


1 CONCEPTUAL PERSPECTIVE RENDERING FROM WILLOW STREET
NOT TO SCALE

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SAN JOSE, CA

CONCEPTUAL PERSPECTIVE DRAWINGS
SCALE: AS NOTED
DRAWN BY: SAE
JOB NO: ALB21
SHEET
A0.1
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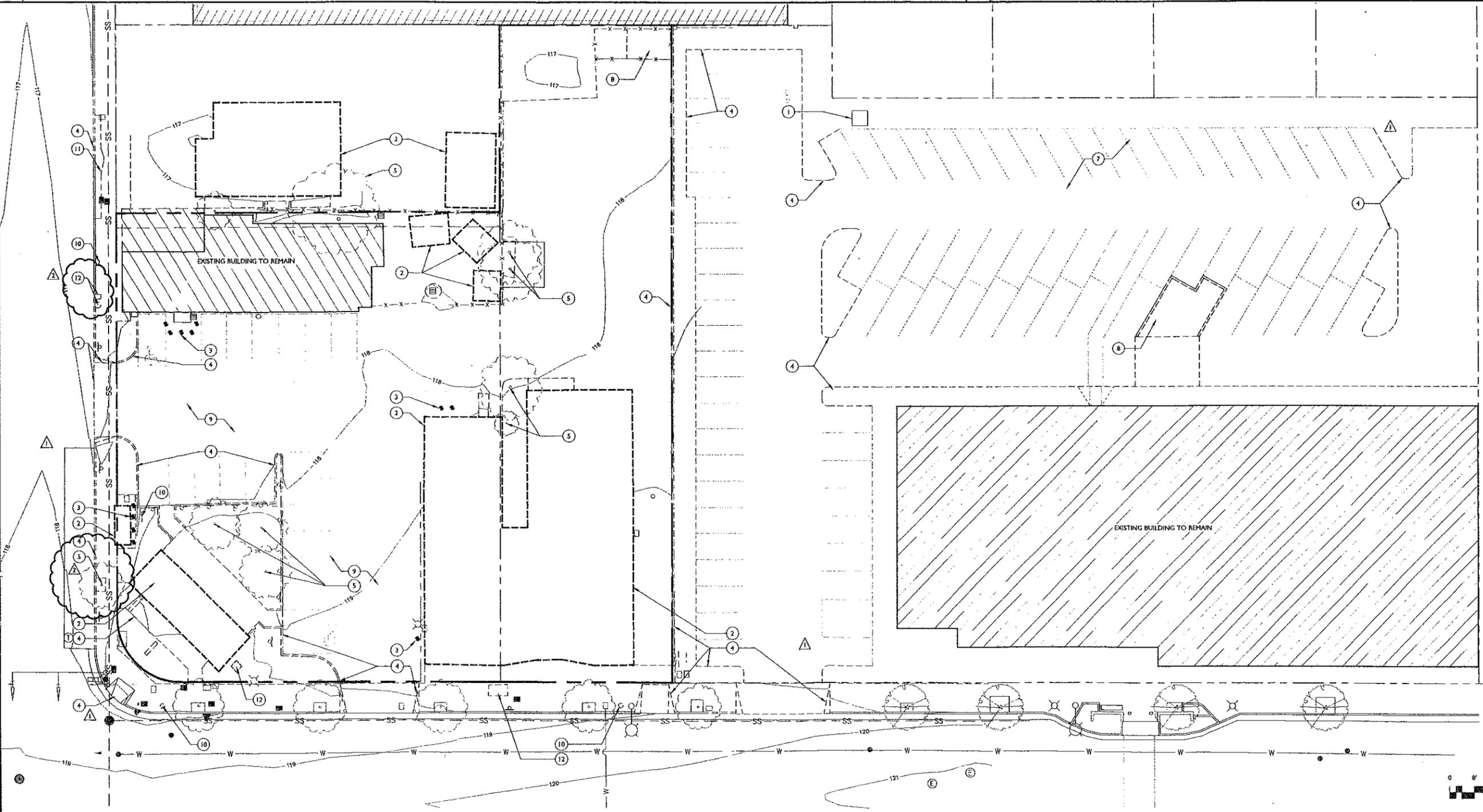
4 EXISTING SITE PHOTOS

- GENERAL DEMOLITION NOTES**
1. PERFORM ALL DEMOLITION OF MATERIALS, COMPONENTS, FIXTURES, AND EQUIPMENT NECESSARY TO PERFORM WORK, IN ADDITION TO THAT SPECIFICALLY CALLED OUT ON THE PLANS.
 2. PROVIDE ALL TEMPORARY SHORING OF STRUCTURAL MEMBERS.
 3. DO NOT PROCEED WITH DEMOLITION PRIOR TO RECEIVING DEMOLITION PERMIT.
 4. PREVENT MOVEMENT OF ADJACENT CONSTRUCTION, PROVIDE AND PLACE BRACING AND BE RESPONSIBLE FOR SAFETY AND SUPPORT OF ADJACENT CONSTRUCTION. CEASE OPERATIONS AND NOTIFY ARCHITECT IF SAFETY OF STRUCTURE APPEARS TO BE IN DANGER.
 5. DETERMINE THE LOCATION OF ALL EXISTING SERVICES PRIOR TO COMMENCEMENT OF DEMOLITION.
 6. INFORM ARCHITECT OF ANY EXISTING SERVICES THAT MAY BE IN CONFLICT WITH REQUIRED DEMOLITION OR CONSTRUCTION OF THIS PROJECT.
 7. DISCONNECT OR REMOVE UTILITY SERVICES AS REQUIRED FOR COMPLETION OF PROJECT.
 8. DISCONNECT, STUB OFF, AND CAP UTILITY SERVICE LINES NOT REQUIRED FOR NEW CONSTRUCTION.
 9. DO NOT REMOVE UTILITIES DISCOVERED DURING DEMOLITION, BUT NOT INDICATED, WITHOUT FIRST DETERMINING PURPOSE FOR UTILITY.
 10. NOTIFY ARCHITECT IMMEDIATELY UPON DISCOVERY OF ANY GRADE BEAMS, BEARING WALLS, OR OTHER ELEMENTS NOT SPECIFICALLY CALLED OUT ON PLANS WHICH MAY BE NOTED "TO BE REMOVED."
 11. REMOVE ALL DEBRIS.
 12. PATCH & REPAIR AS NECESSARY ALL EXISTING CONDITIONS TO REMAIN.

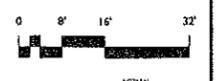
3 DEMOLITION NOTES

- KEY NOTES:**
- 1 (E) 2000 AMP, 120/208, 3 PHASE, 4 WIRE ELECTRICAL PANEL TO REMAIN
 - 2 REMOVE ALL FLOORS, STAIR, WALLS, DOORS, FINISHES, CEILINGS, DUCTS & ROOF
 - 3 REMOVE BOLLARDS
 - 4 REMOVE CURB OR CURB CUT & ADJACENT CONCRETE SIDEWALKS.
 - 5 REMOVE (E) TREE
 - 6 (E) PARKING LOT, NEW STRIPING AND TOPPING.
 - 7 DEMO PARKING/PAVING AS REQUIRED FOR NEW RAMP AND PARKING DECK FOUNDATIONS.
 - 8 REMOVE TRASH ENCLOSURE
 - 9 REMOVE ALL PAVING MATERIALS
 - 10 REMOVE EXISTING SITE FURNITURE, REPLACE W/ NEW AS SHOWN ON PROPOSED SITE PLANS.
 - 11 REMOVE / DEMO (E) PLANTING AREA. SEE A1.1 FOR (N) CONSTRUCTION
 - 12 REMOVE / DEMO UTILITY BOXES

2 DEMOLITION KEY NOTES



1 EXISTING/DEMOLITION SITE PLAN
SCALE: 1/16" = 1'-0" (A4 X 24)



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PLANNED DEVELOPMENT (PD)
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SAN JOSE, CA

EXISTING/
DEMOLITION
SITE PLAN
SCALE 1/16" = 1'-0"
DRAWN BY: SAE
JOB NO: ALB11
SHEET

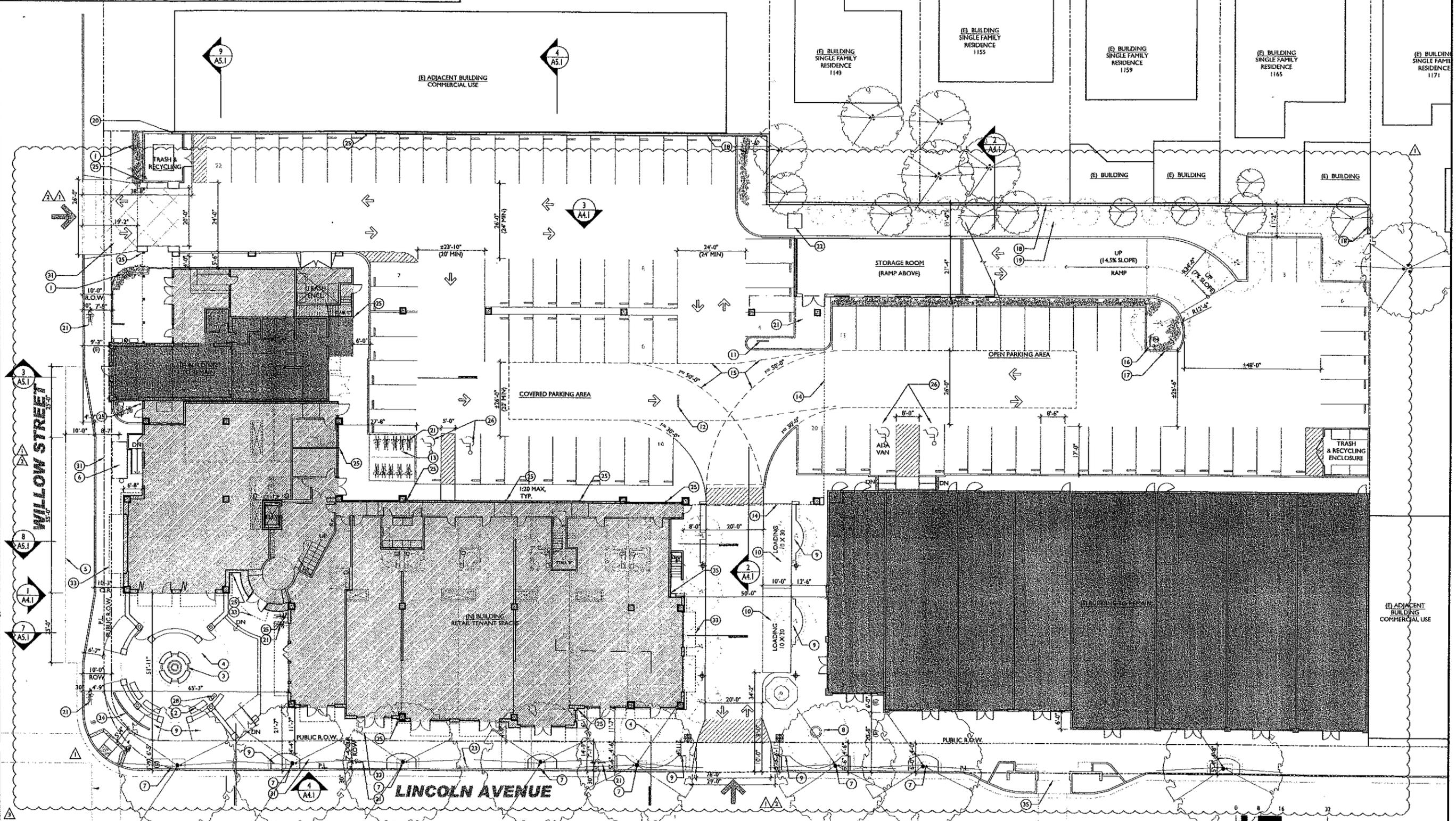
A1.0

- 1 LOW WALL W/ PLANTER
- 2 BENCH SEAT
- 3 PROPOSED FOUNTAIN
- 4 DECORATIVE PAVING PATTERN
- 5 (N) BUS PAD
- 6 (N) RELOCATED BUS SHELTER
- 7 (E) TREE TO REMAIN
- 8 (N) SIDEWALK PAVERS @ (E) SIDEWALK
- 9 LANDSCAPING AT GRADE *
- 10 LOADING ZONE
- 11 SIGN THAT READS: "NO LEFT TURN" OR EQUIV.
- 12 DOUBLE SIDED SIGN READS: "STOP" & "DO NOT ENTER"
- 13 UNDERGROUND GREASE INTERCEPTOR
- 14 EDGE OF (N) PARKING DECK (DASHED ABOVE)
- 15 REQUIRED PATH OF TRAVEL FOR EMERGENCY VEHICLES / FIRE ACCESS. MIN. 13'-6" CLR. HEIGHT / 14'-6" PROPOSED.
- 16 SIGN "TURN LEFT TO UPPER PARKING. CAUTION 2-WAY TRAFFIC"
- 17 ISLAND PLANTER W/ CURB
- 18 (N) CONCRETE WALL @ PROPERTY LINE
- 19 LANDSCAPE STRIP W/ (N) TREES
- 20 (N) MASONRY WALL
- 21 BICYCLE RACK
- 22 (E) ABOVE GROUND UTILITIES TO REMAIN
- 23 (E) POST MOUNTED SIGN
- 24 (E) IN GROUND UTILITIES TO REMAIN
- 25 (N) DOWNSPOUT
- 26 (N) ACCESSIBLE PARKING STALL
- 27 (N) SLOPED ACCESS TO SIDEWALK
- 28 (N) DRINKING FOUNTAIN
- 29 ±6'-0" HT. PERIMETER WALL @ PARKING DECK
- 30 (N) TRANSFORMER PAD
- 31 STAMPED COLORED CONCRETE
- 32 TRAFFIC SIGN
- 33 AWNING ABOVE OR BELOW
- 34 PLAZA SIGN ON LOW WALL W/ LANDSCAPE
- 35 (E) CROSSWALK W/ DECORATIVE STEEL ARCH & CONCRETE POSTS

2 KEY NOTES

BLEWETT AVE.

NOTE:
*PUBLIC R.O.W. IMPROVEMENTS SHOWN FOR REFERENCE ONLY AND UNDER DEFERRED PERMIT SUBMITTAL.



1 GROUND FLOOR SITE PLAN & FLOOR PLAN
SCALE: 1/16"=1'-0" (24 X 36 SHEET)

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PLANNED DEVELOPMENT (PD)
1104 LINCOLN AVE.
SAN JOSE, CA

SITE & GROUND FLOOR PLAN
SCALE: 1/16"=1'-0"
DRAWN BY: SAE/DA
JOB NO:
SHEET

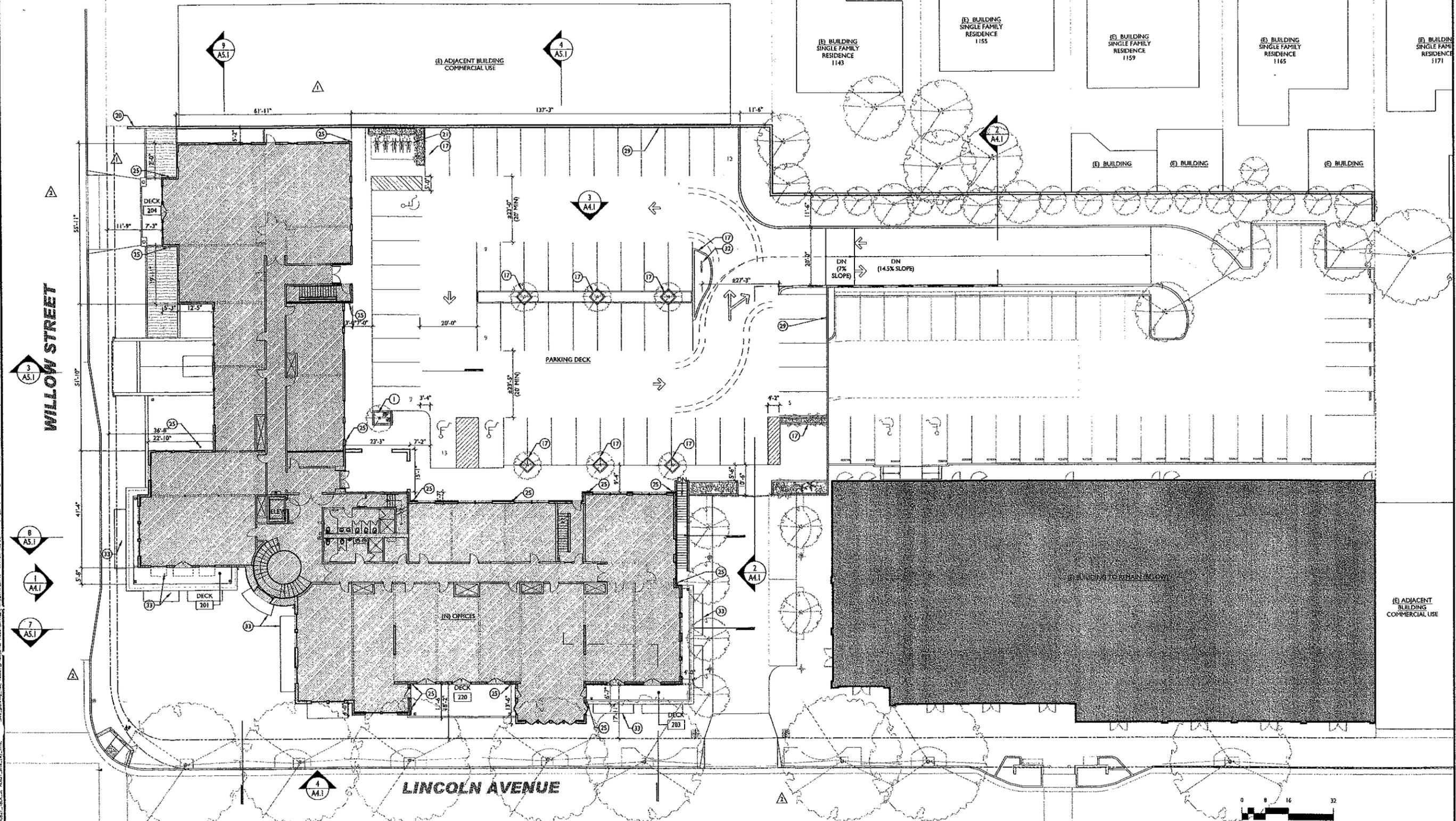
AI.I
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- KEY NOTES:**
- 1 LOW WALL W/ PLANTER
 - 2 BENCH SEAT
 - 3 PROPOSED FOUNTAIN
 - 4 DECORATIVE PAVING PATTERN
 - 5 (N) BUS PAD
 - 6 (N) RELOCATED BUS SHELTER
 - 7 (E) TREE TO REMAIN
 - 8 (N) SIDEWALK PAVERS @ (E) SIDEWALK
 - 9 LANDSCAPING AT GRADE*
 - 10 LOADING ZONE
 - 11 SIGN THAT READS: "NO LEFT TURN" OR EQUIV.
 - 12 DOUBLE SIDED SIGN READS: "STOP" & "DO NOT ENTER"
 - 13 UNDERGROUND GREASE INTERCEPTOR
 - 14 EDGE OF (N) PARKING DECK (DASHED ABOVE)
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 - 19 LANDSCAPE STRIP W/ (N) TREES
 - 20 (N) MASONRY WALL
 - 21 BICYCLE RACK
 - 22 (E) ABOVE GROUND UTILITIES TO REMAIN
 - 23 (E) POST MOUNTED SIGN
 - 24 (E) IN GROUND UTILITIES TO REMAIN
 - 25 (N) DOWNSPOUT
 - 26 (N) ACCESSIBLE PARKING STALL
 - 27 (N) SLOPED ACCESS TO SIDEWALK
 - 28 (N) DRINKING FOUNTAIN
 - 29 26'-0" HT. PERIMETER WALL @ PARKING DECK
 - 30 (N) TRANSFORMER PAD
 - 31 STAMPED COLORED CONCRETE
 - 32 TRAFFIC SIGN
 - 33 AWNING ABOVE OR BELOW
 - 34 PLAZA SIGN ON LOW WALL W/ LANDSCAPE
 - 35 (E) CROSSWALK W/ DECORATIVE STEEL ARCH & CONCRETE POSTS

2 KEY NOTES

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1104 LINCOLN AVE.
 SAN JOSE, CA

PARKING DECK
 & SECOND FLOOR
 PLAN

SCALE: AS NOTED
 DRAWN BY: SAE / DA
 JOB NO: ALB21
 SHEET

A1.2

SECOND LEVEL PARKING DECK PLAN & SECOND FLOOR PLAN



ACTUAL NORTH

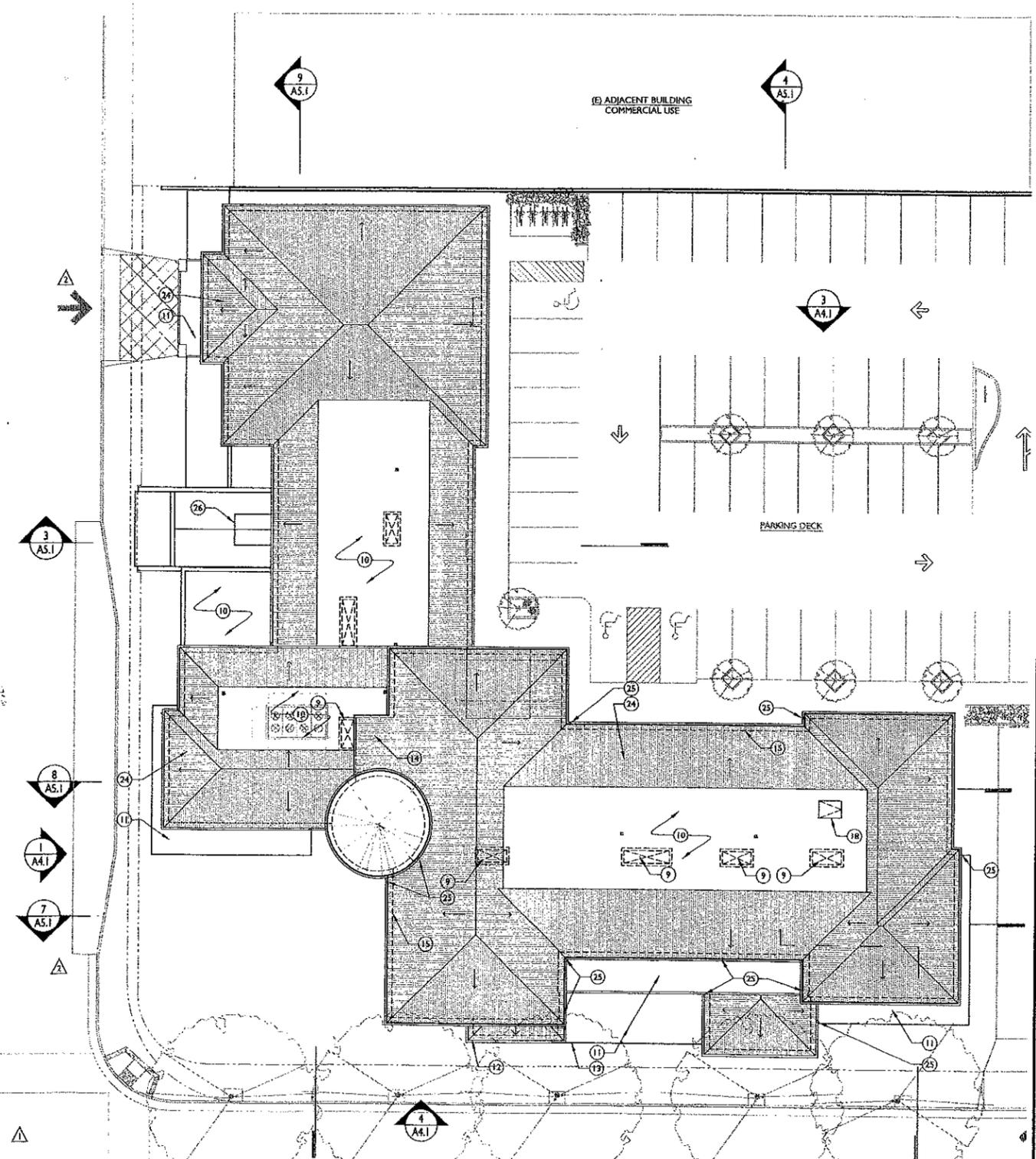
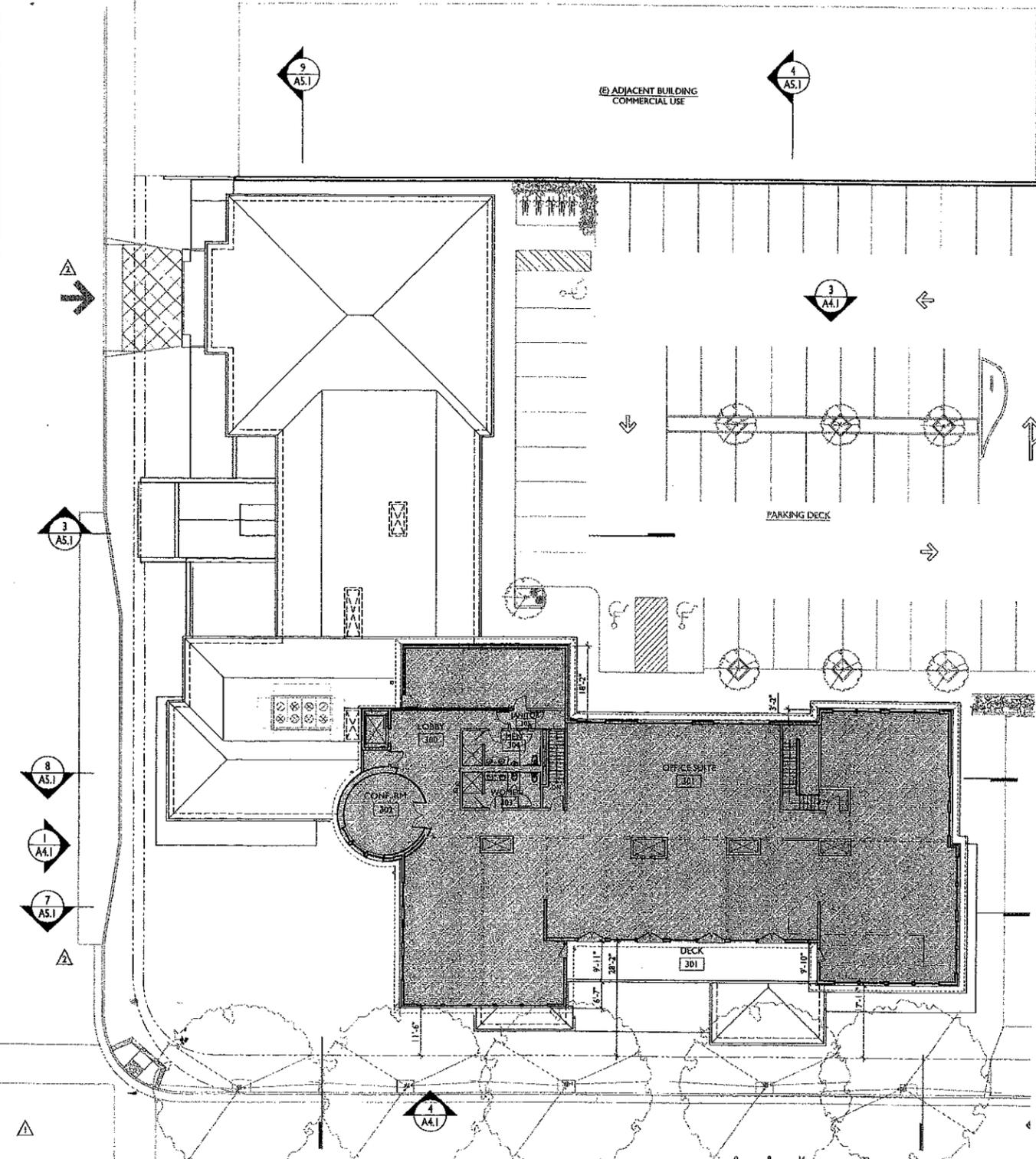
KEY NOTES:

- 1 VERTICAL SHAFT ABOVE 3'-0" x 7'-0" CLEAR TYP. EDGE OF STAIR ABOVE
- 2 MAILBOXES
- 3 VERTICAL SHAFT 3'-0" x 7'-0" CLEAR TYP. GUARDRAIL
- 4 CHANGE IN CEILING STRUCTURE
- 5 LOW WALL
- 6 AREA OF EVACUATION ASSISTANCE PROPOSED SHAFT LOCATION BELOW FLAT ROOF
- 7 DECK BELOW
- 8 ROOF EDGE BELOW
- 9 EDGE OF CORNICE BELOW
- 10 ELEVATOR PENTHOUSE
- 11 OUTLINE OF WALL BELOW
- 12 ELEVATOR MECHANICAL ROOM. SPLIT SYSTEM W/ MEZZANINE ABOVE.
- 13 OUTLINE OF ADJACENT BUILDING
- 14 ROOF ACCESS STAIR
- 15 OTIS LVM2500E ELEVATOR OR EQUAL
- 16 BUILDING DIRECTORY
- 17 ALLOW REQ'D CLEAR SPACE PER 61A0.3
- 18 DRINKING FOUNTAIN. KOHLER SIERRA DRINKING FOUNTAIN MODEL #5250.
- 19 ROOF ACCESS LADDER
- 20 CONCRETE TILE ROOF (TYP.)
- 21 COPPER GUTTER AND DOWN SPOUT (TYP.)
- 22 (S) SKYLIGHT

3 ROOF PLAN KEY NOTES

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PLANNED DEVELOPMENT (PD)
1104 LINCOLN AVE.
SAN JOSE, CA

THIRD FLOOR & ROOF PLAN
SCALE: 1/16"=1'-0"
DRAWN BY: SAE / DA
JOB NO: ALB21
SHEET

AI.3

REVISION:	DATE:
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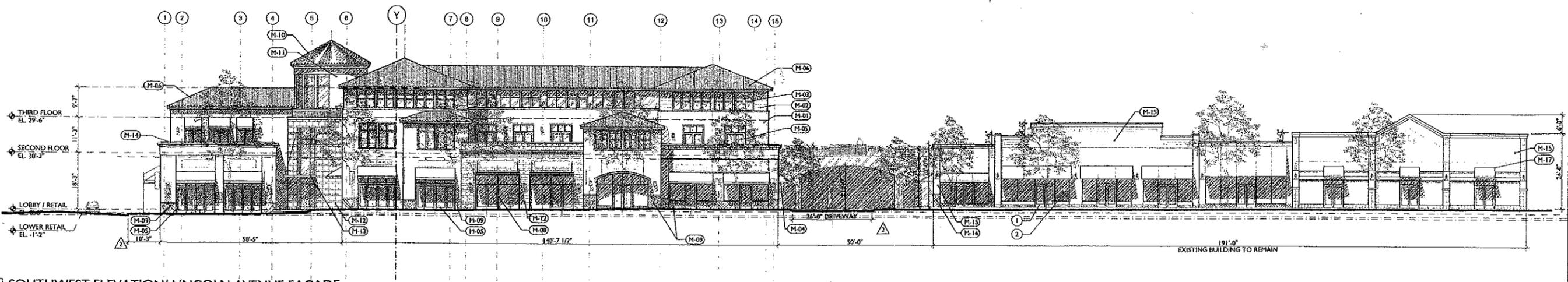
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PLANNED DEVELOPMENT (PD)
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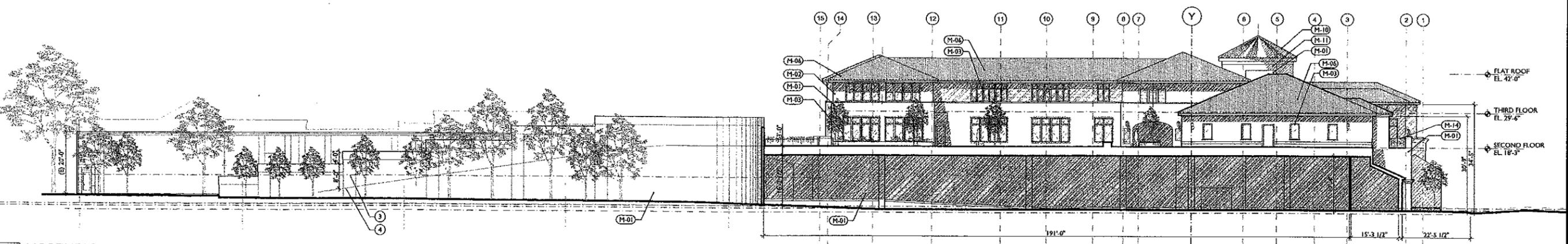
PROPOSED
EXTERIOR
ELEVATIONS

SCALE: AS NOTED
DRAWN BY: SAE
JOB NO: ALB21
SHEET

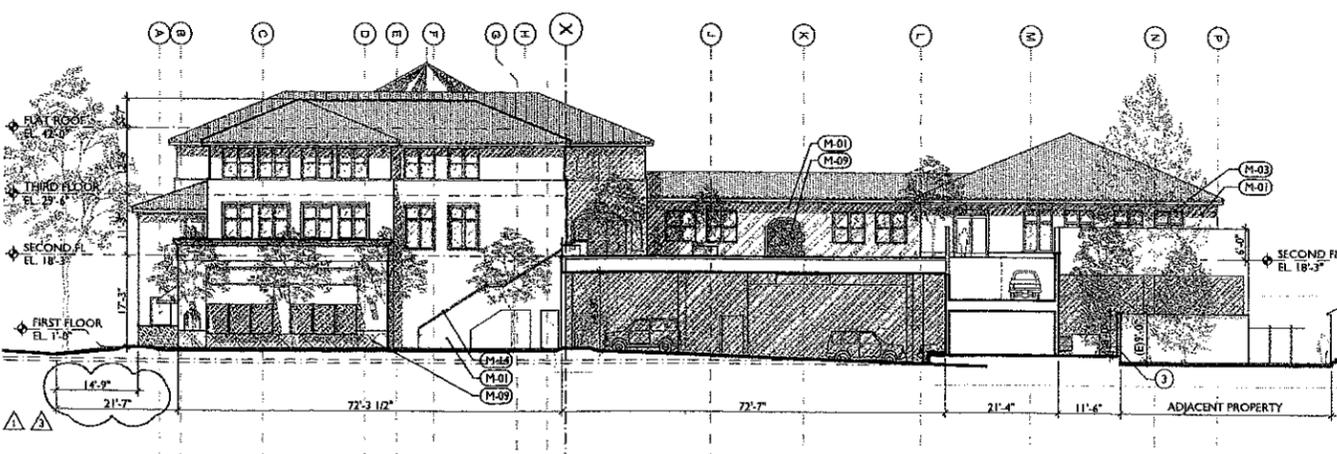
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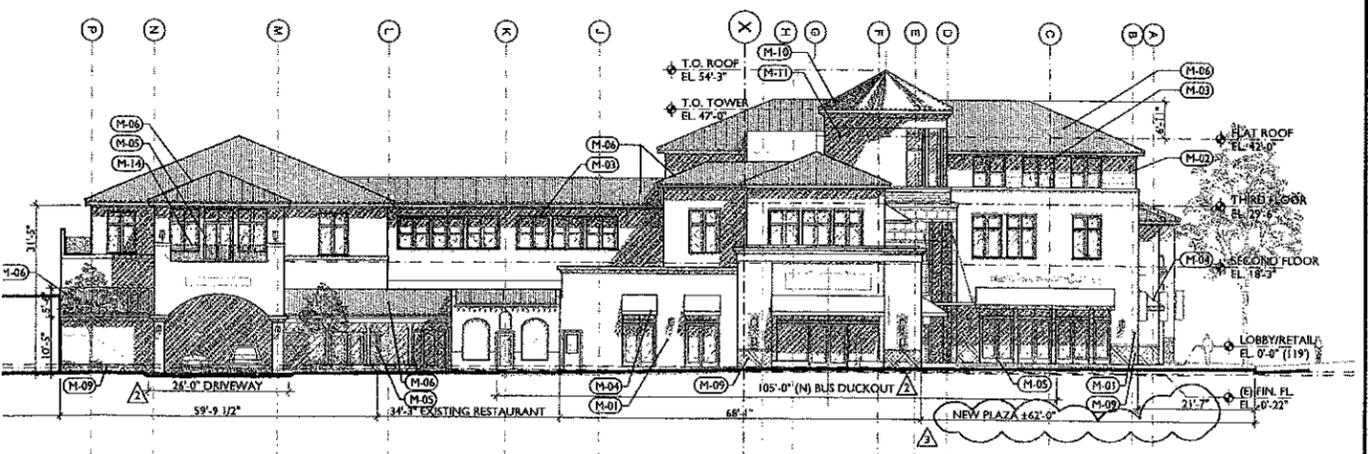
4 SOUTHWEST ELEVATION/ LINCOLN AVENUE FACADE
SCALE: 1/16"=1'-0"



3 NORTHEAST ELEVATION
SCALE: 1/16"=1'-0"



2 SOUTHEAST ELEVATION
SCALE: 1/16"=1'-0"



1 NORTHWEST ELEVATION/ WILLOW STREET FACADE
SCALE: 1/16"=1'-0"

FINISH	DESCRIPTION	SPECIFICATION	FINISH	DESCRIPTION	SPECIFICATION
M-01	STUCCO (TYP.)	MFR: T.B.D. COLOR: T.B.D.	M-08	ALUMINUM STOREFRONT SYSTEMS (TYP.)	MFR: T.B.D. COLOR: T.B.D.
M-02	HORIZONTAL SIDING	MFR: T.B.D. COLOR: T.B.D.	M-09	WAINSCOT TILE BASE (VARIES)	MFR: T.B.D. COLOR & PATTERN: T.B.D.
M-03	ALUMINUM WINDOWS (TYP.)	MFR: T.B.D. COLOR: T.B.D.	M-10	METAL ROOF	MFR: T.B.D. COLOR: T.B.D.
M-04	CANVAS AWNING (TYP.)	MFR: T.B.D. COLOR: T.B.D.	M-11	ALUMINUM PANELS	MFR: T.B.D. COLOR: T.B.D.
M-05	WOOD DOGGS (TYP.)	MFR: T.B.D. COLOR: T.B.D.	M-12	STONE OR TILE CLADDING	MFR: T.B.D. COLOR & PATTERN: T.B.D.
M-06	CONCRETE MISSION STYLE ROOF TILES (TYP.)	MFR: T.B.D. COLOR: T.B.D.	M-13	GLASS AND STEEL ENTRY CANOPY	MFR: T.B.D. COLOR: T.B.D.
M-07	GLASS AND STEEL AWNING	MFR: T.B.D. COLOR: T.B.D.	M-14	STEEL GUARDRAIL (TYP.)	MFR: T.B.D. COLOR: T.B.D.

6 EXTERIOR FINISH MATERIAL SCHEDULE

FINISH	DESCRIPTION	SPECIFICATION
M-15	(N) PAINT AT (E) STUCCO	MFR: T.B.D. COLOR: T.B.D.
M-16	(N) TILE AT (E) LOCATION	MFR: T.B.D. COLOR: T.B.D.
M-17	(N) CANVAS AWNING AT (E) LOCATION	MFR: T.B.D. COLOR: T.B.D.

MATERIAL FINISH NOTES:
1. CONTACT ARCHITECT PRIOR TO PURCHASING PAINT. ARCHITECT TO PROVIDE (2) ALTERNATE SHADES FOR EACH SPECIFIED PAINT COLOR. COLOR TO BE BRUSHED OUT ON BUILDING BEFORE FINAL SELECTION. ARCHITECT TO REVIEW AND APPROVE BRUSH OUTS PRIOR TO PAINTING.
2. CONTRACTOR TO SUPPLY A SAMPLE AND CUTSHEETS OF ALL MATERIALS SPECIFIED. ARCHITECT TO REVIEW AND APPROVE MATERIAL SUBMITTALS BEFORE CONTRACTOR ORDERS AND/OR PURCHASES.

5 ELEVATION KEY NOTES

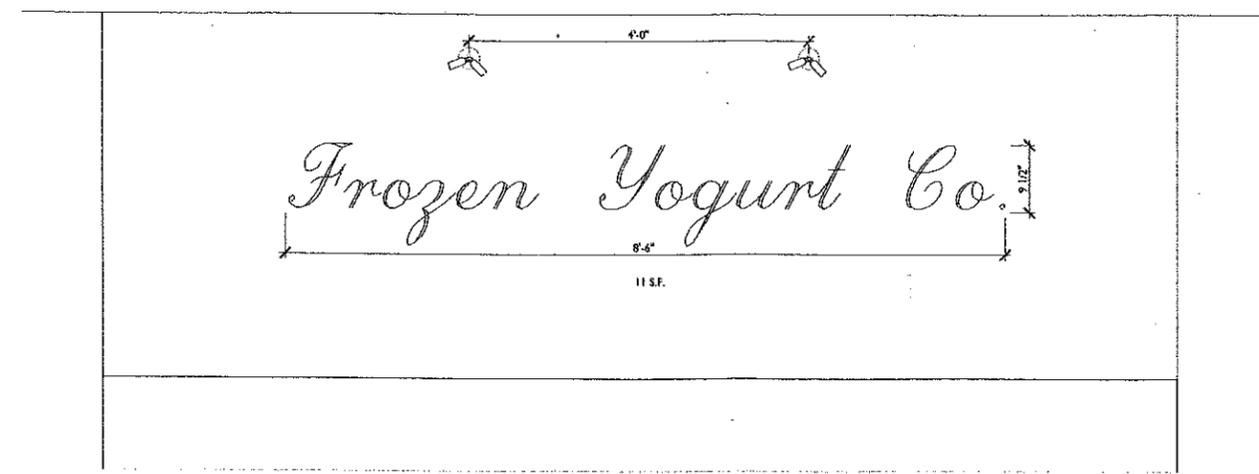
- ELEVATION KEY NOTES:**
- REPLACE EXISTING DOORS AND DOOR HARDWARE TO MATCH NEW AT ADJACENT BUILDING, TYP. @ ALL STOREFRONTS.
 - REPLACE EXISTING LIGHT FIXTURES TO MATCH NEW AT ADJACENT BUILDING, TYP.
 - EXISTING STUCCO WALL AT PROPERTY LINE.
 - RAMP SLOPE BEYOND

5 ELEVATION KEY NOTES

1. AS NOTED OR AS NOTED ALBERTA 2. AS NOTED ALBERTA 3. AS NOTED ALBERTA 4. AS NOTED ALBERTA 5. AS NOTED ALBERTA 6. AS NOTED ALBERTA 7. AS NOTED ALBERTA

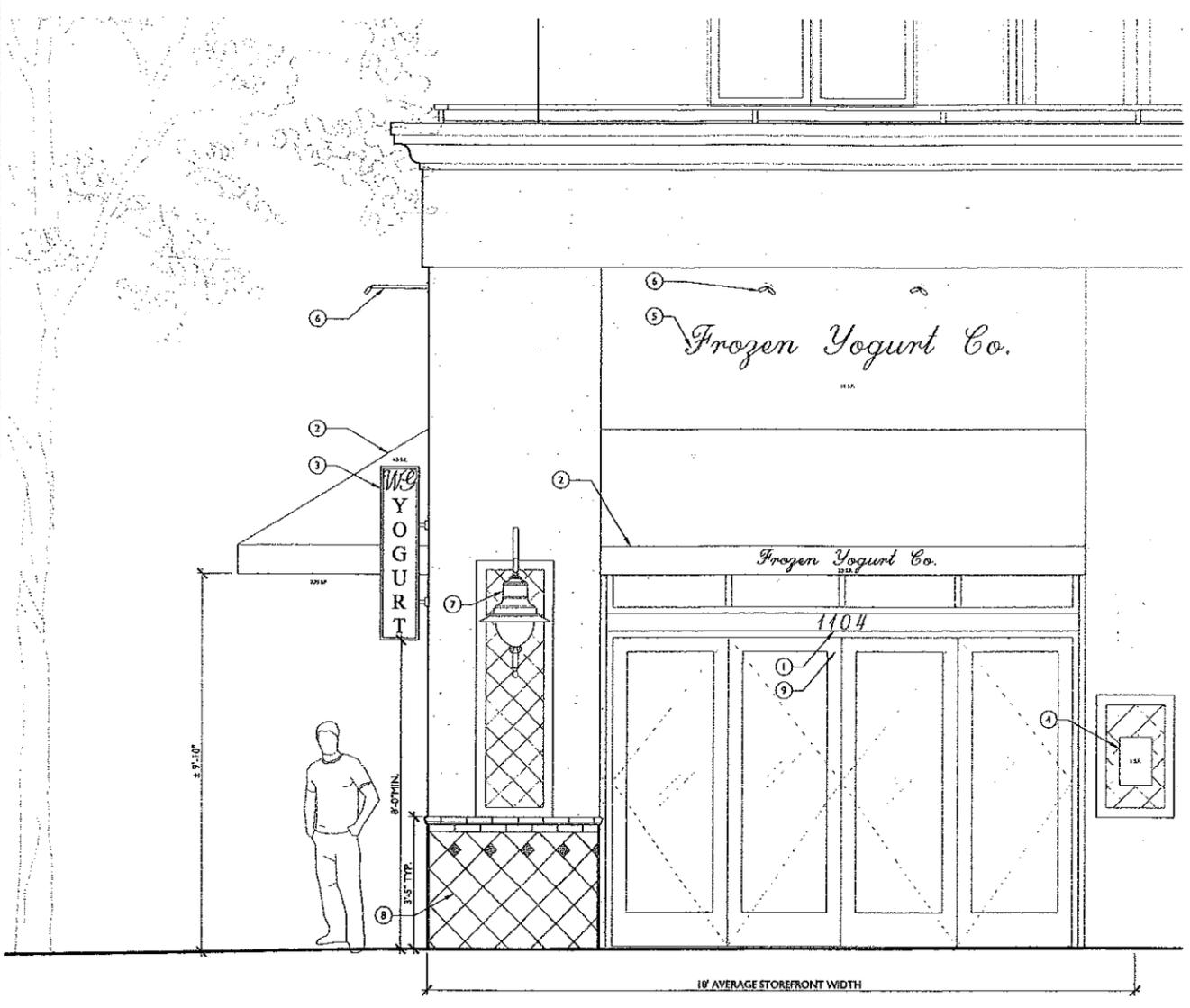
6 SCALE: 1"=1'-0"

5 PROPOSED VEHICULAR SIGNAGE
SCALE: 1"=1'-0"

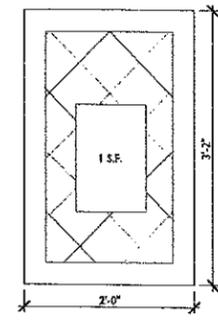


7 SCALE: 1"=1'-0"

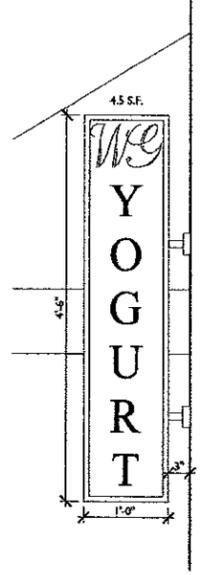
1 ELEMENTS OF A CONCEPTUAL STOREFRONT ELEVATION
SCALE: 1/2"=1'-0"



4 PROPOSED SMALL SIGN DETAIL
SCALE: 1"=1'-0"



3 PROPOSED BLADE SIGN
SCALE: 1"=1'-0"



2 KEY LEGEND & NOTES
SCALE: N.T.S.

- 1 FINISH WOOD BEAM AT STOREFRONT
- 2 CANVAS AWNING
- 3 BLADE SIGN, SEE DETAIL 3.
- 4 SMALL SIGNAGE, SEE DETAIL 4
- 5 VEHICULAR SIGNS, SEE DETAIL 5
- 6 EXTERIOR SIGN LIGHTING
- 7 EXTERIOR WALL MOUNTED SCONCE, SEE LIGHTING DWGS FOR FIXTURE DESIGN AND LOCATION
- 8 DECORATIVE TILE WAJNSCOT
- 9 ALUMINUM STOREFRONT WITH WOOD DOORS

NOTES:

- FOR ADDITIONAL SIGNAGE GUIDELINES REFER TO THE CITY OF SAN JOSE 'COMMERCIAL DESIGN GUIDELINES' AND 'DOWNTOWN WILLOW GLEN REVITALIZATION STRATEGY PROJECT FOR THE LINCOLN AVENUE NEIGHBORHOOD BUSINESS DISTRICT'.
- SIGNS SHOULD BE CONSISTENT IN SIZE AND STYLE FOR MULTIPLE TENANTS WITHIN A BUILDING OR BUILDING AREA.
- SIGNS SHOULD BE APPROPRIATE TO THE BUILDING STYLE AND SHOULD NOT CONCEAL SIGNIFICANT ARCHITECTURAL DETAILS.
- ALLOWABLE SIGN AREA SHALL BE EQUAL TO THE LINEAR STORE FRONTAGE MULTIPLIED BY 1.5 FEET. 50% OF ALLOWABLE AREA SHOULD BE AUTO-ORIENTED SIGNAGE, 30% SHOULD BE PEDESTRIAN ORIENTED SIGNAGE, AND 20% SHOULD BE USED WHERE NEEDED.
- SIGNS SHALL BE WELL-LIT AND EASY TO READ.
- AWNINGS SHALL BE WELL-MAINTAINED AND REPLACED WHEN FADED AND / OR TORN.

REVISION:	DATE:
PDC PERMIT	04.24.09
PD PERMIT	05.07.09
REV #1	09.18.09
REV #2	11.23.09

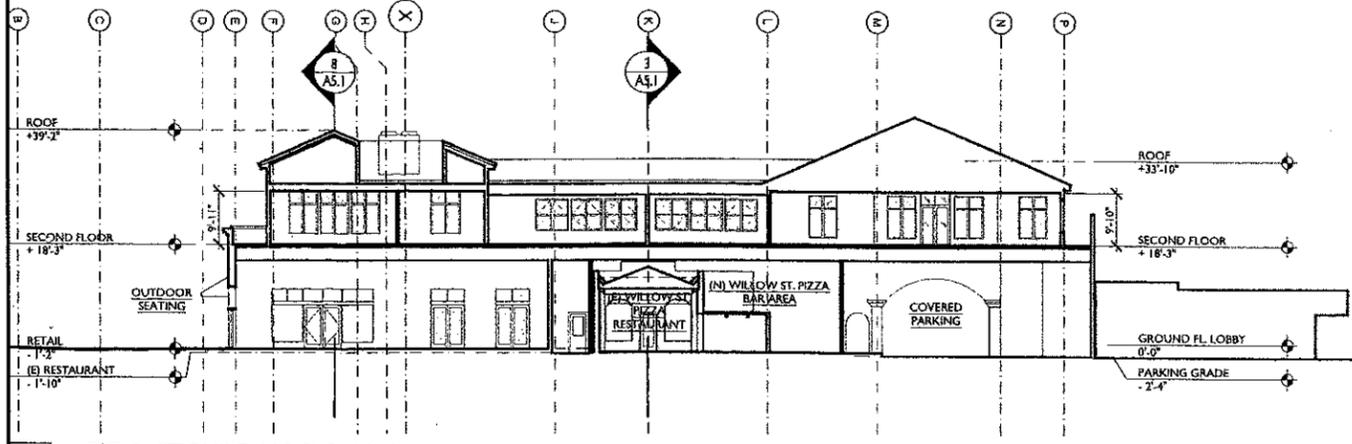
THIS PLAN IS A CONCEPTUAL DESIGN AND IS NOT FOR CONSTRUCTION UNLESS THEY ARE THE ARCHITECT'S BELIEF AND THE ARCHITECT HAS BEEN ADVISED BY THE CITY OF SAN JOSE THAT THE CITY OF SAN JOSE HAS REVIEWED AND APPROVED THIS PLAN. THE ARCHITECT'S BELIEF AND THE ARCHITECT'S BELIEF IS NOT A GUARANTEE OF ANY KIND. THE ARCHITECT'S BELIEF IS NOT A GUARANTEE OF ANY KIND. THE ARCHITECT'S BELIEF IS NOT A GUARANTEE OF ANY KIND.

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 phone: 510.841.3555 fax: 510.841.1122
 kahndesignassociates.com

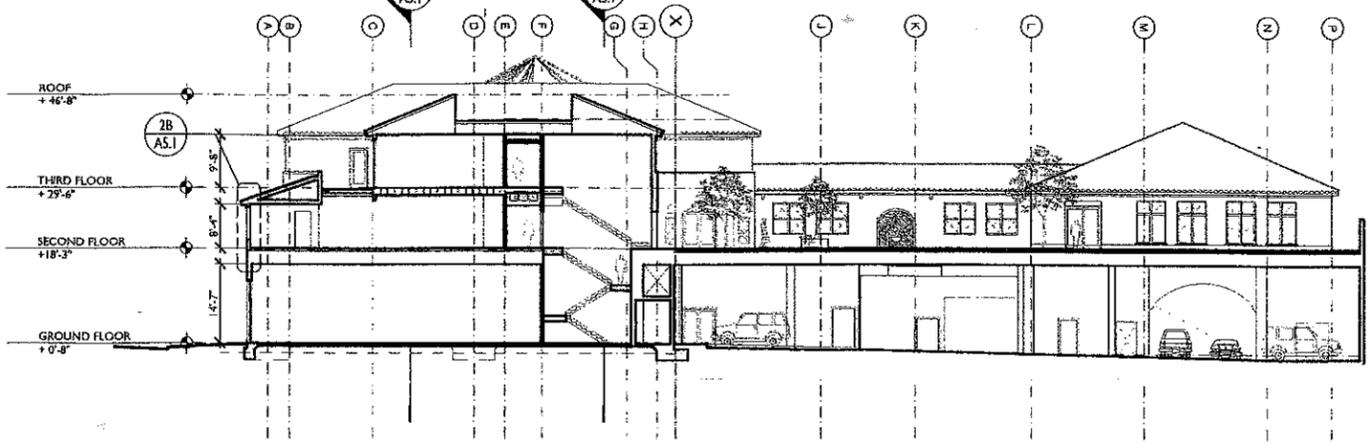
PLANNED DEVELOPMENT (PD)
1104 LINCOLN AVE.
 SAN JOSE, CA

PROPOSED
 DESIGN
 DETAILS
 SCALE: AS NOTED
 DRAWN BY: SAE
 JOB NO: ALB21
 SHEET

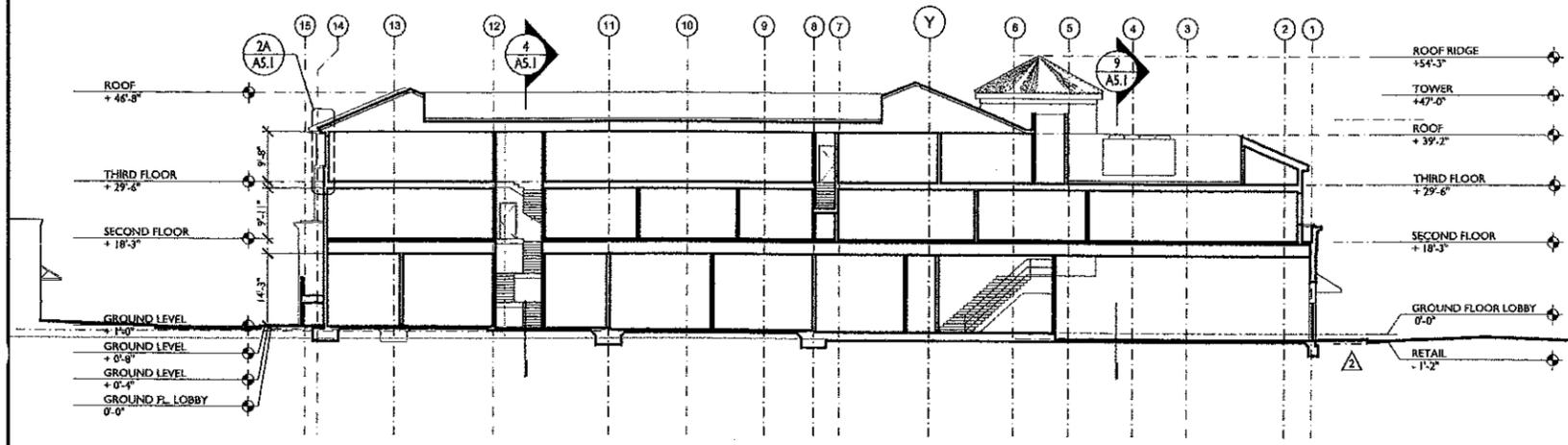
A4.2



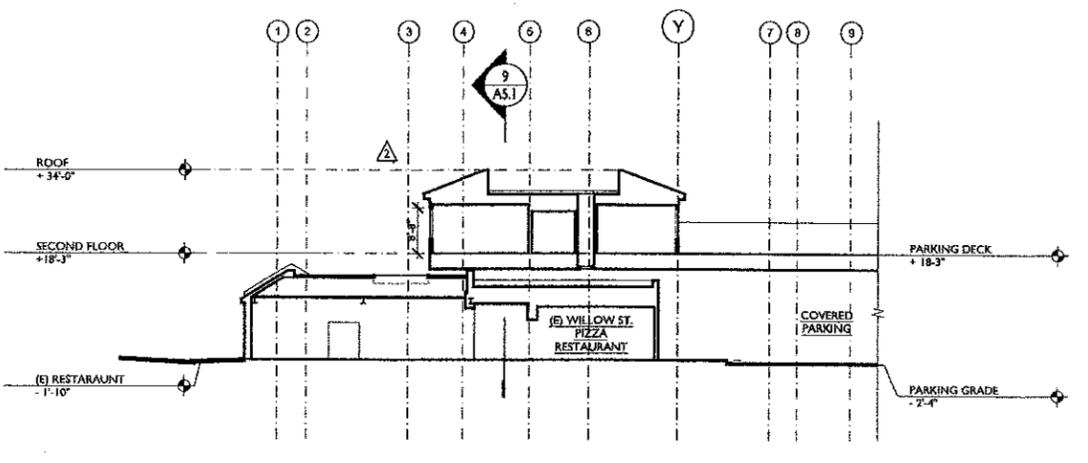
9 WEST-EAST BUILDING SECTION
SCALE: 1/16"=1'-0"



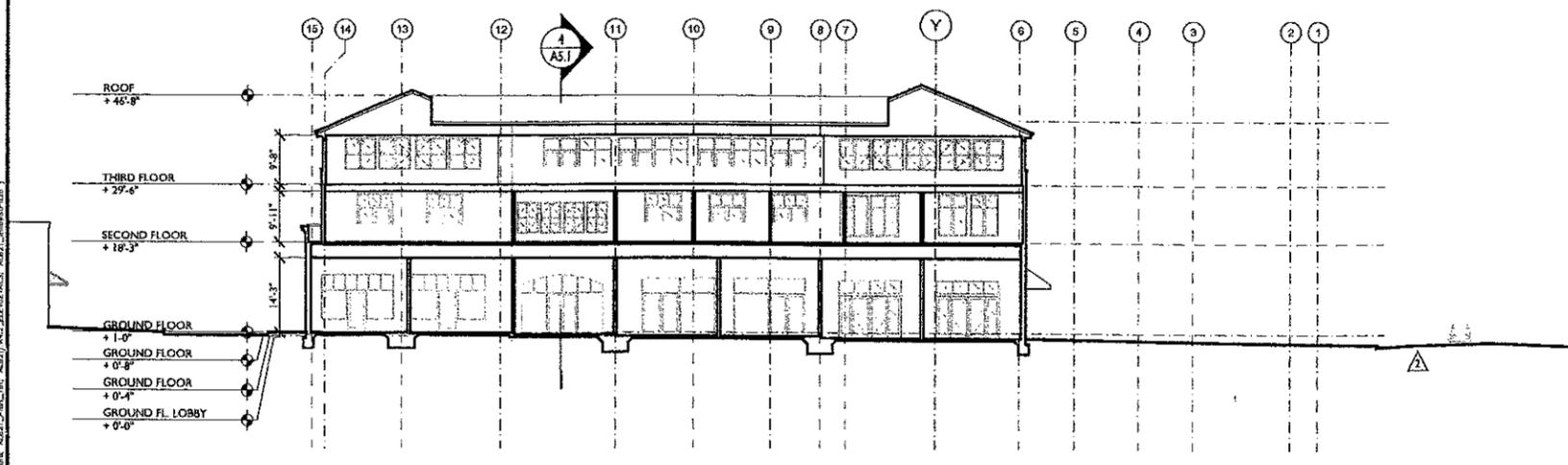
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SCALE: 1/16"=1'-0"



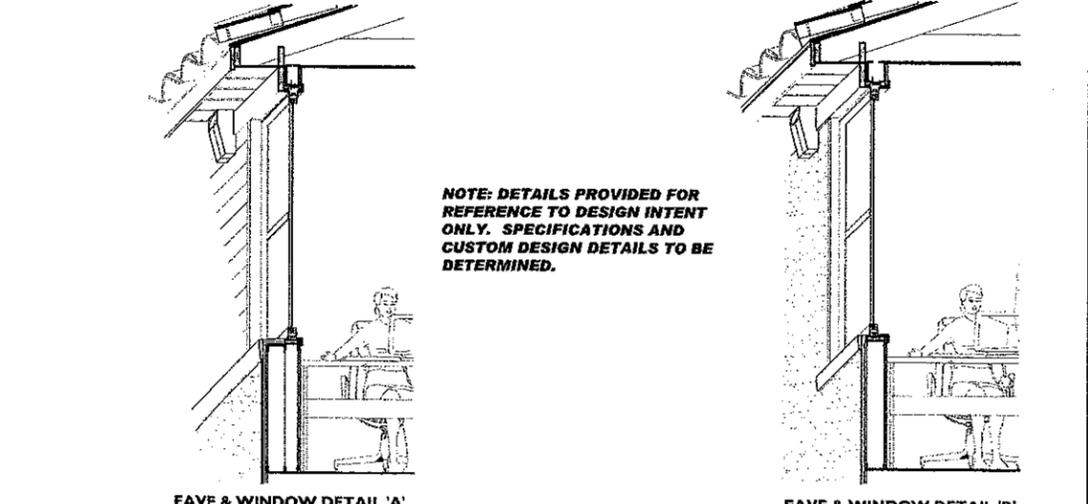
8 SOUTH-NORTH BUILDING SECTION
SCALE: 1/16"=1'-0"



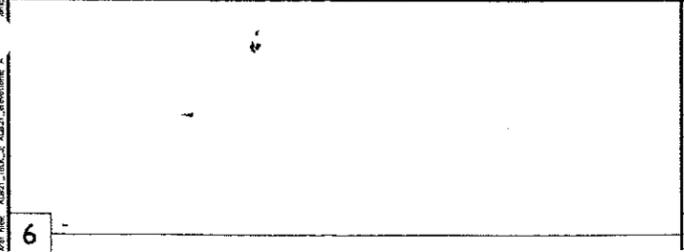
3 NORTH-SOUTH BUILDING SECTION
SCALE: 1/16"=1'-0"



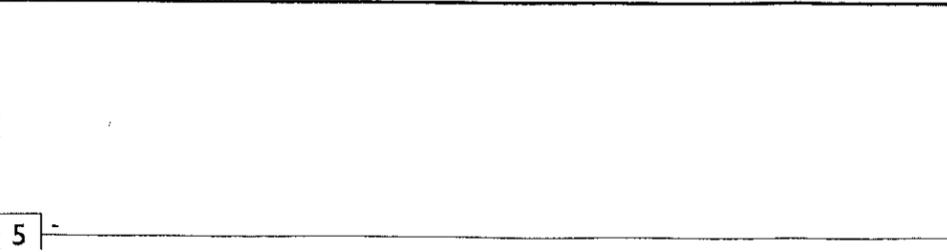
7 SOUTH-NORTH BUILDING SECTION
SCALE: 1/16"=1'-0"



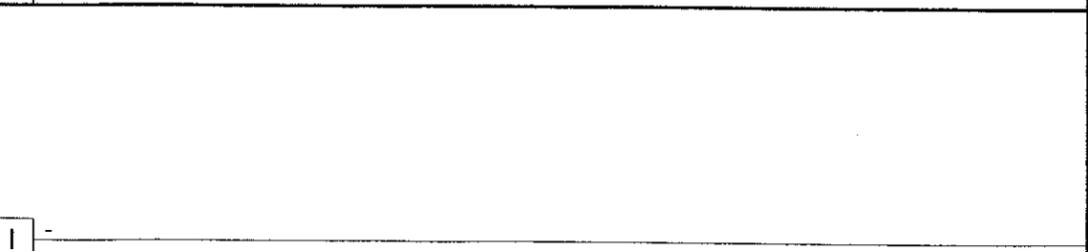
2 CONCEPTUAL DETAILS
SCALE: 1/2"=1'-0"



6



5



1

REVISION:	DATE:
PDC PERMIT	04.24.09
PD PERMIT	05.07.09
REV #1	09.18.09
REV #2	11.23.09
REV #3	01.04.10

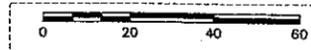
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kahn@kdaassociates.com

PLANNED DEVELOPMENT (PD)
1104 LINCOLN AVE.
SAN JOSE, CA

BUILDING SECTIONS
SCALE: AS NOTED
DRAWN BY: SAE
JOB NO: ALB21
SHEET
A5.1

BLEWETT AVENUE



GAGLIARDI BROTHERS
429-06-073
EXISTING COMMERCIAL BUILDING

MARSHMAN
429-06-005

ROWE
429-06-006
EXISTING SINGLE FAMILY RESIDENTIAL

CURLEY
429-06-007

DOMINGUEZ
429-06-008

Basis of Bearings

The bearing N 29° 13' 33" W of the centerline of Lincoln Avenue as shown on that certain Record of Survey map recorded February 27, 1990 in Book 611 of Maps at page 14 in the office of the Recorder of Santa Clara County, State of California and as found monumented was taken as basis of bearings for this map.

BENCHMARK

Benchmark No. 799
Elevation 118.45 (NGVD 29)

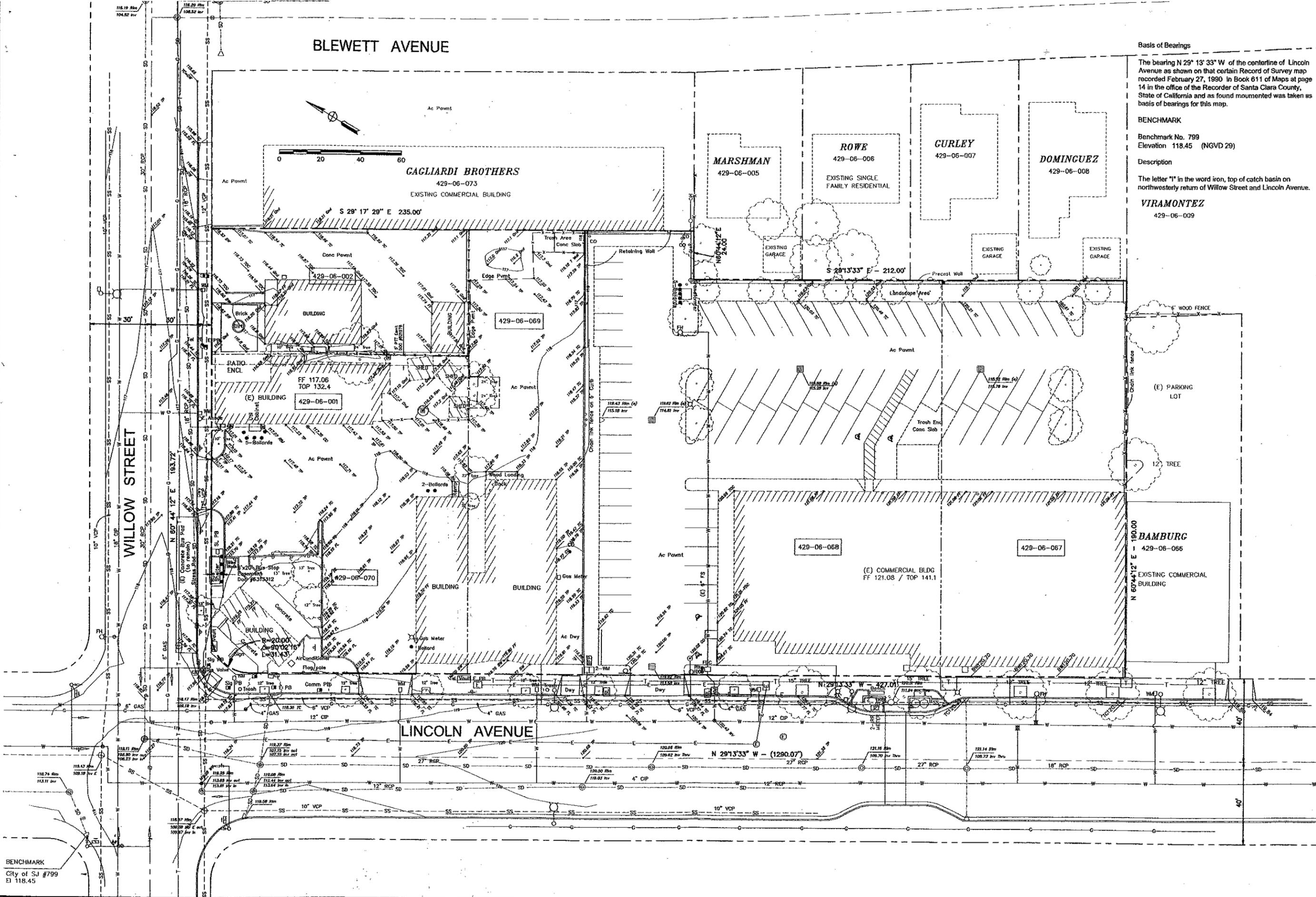
Description

The letter "I" in the word iron, top of catch basin on northwesterly return of Willow Street and Lincoln Avenue.

VIRAMONTEZ
429-06-009

WILLOW STREET

LINCOLN AVENUE



BENCHMARK
City of SJ #799
E 118.45

DATE	06/07/09
DRAWN BY	LO
CHECKED BY	LU
SCALE	1" = 20'

DATE	REVISION
12/29/08	REVISION # 3
09/07/08	REVISION # 1

APPROVED BY: [Signature]

3170 WILLIAMS ROAD
SAN JOSE, CA 95128
(408) 241-1000

A DIVISION OF
CONSULTING ENGINEERS AND SURVEYORS
LAND PLANNERS

ALLIED ENGINEERING COMPANY

TOPOGRAPHIC SURVEY
1104 LINCOLN AVENUE
SAN JOSE, CALIFORNIA

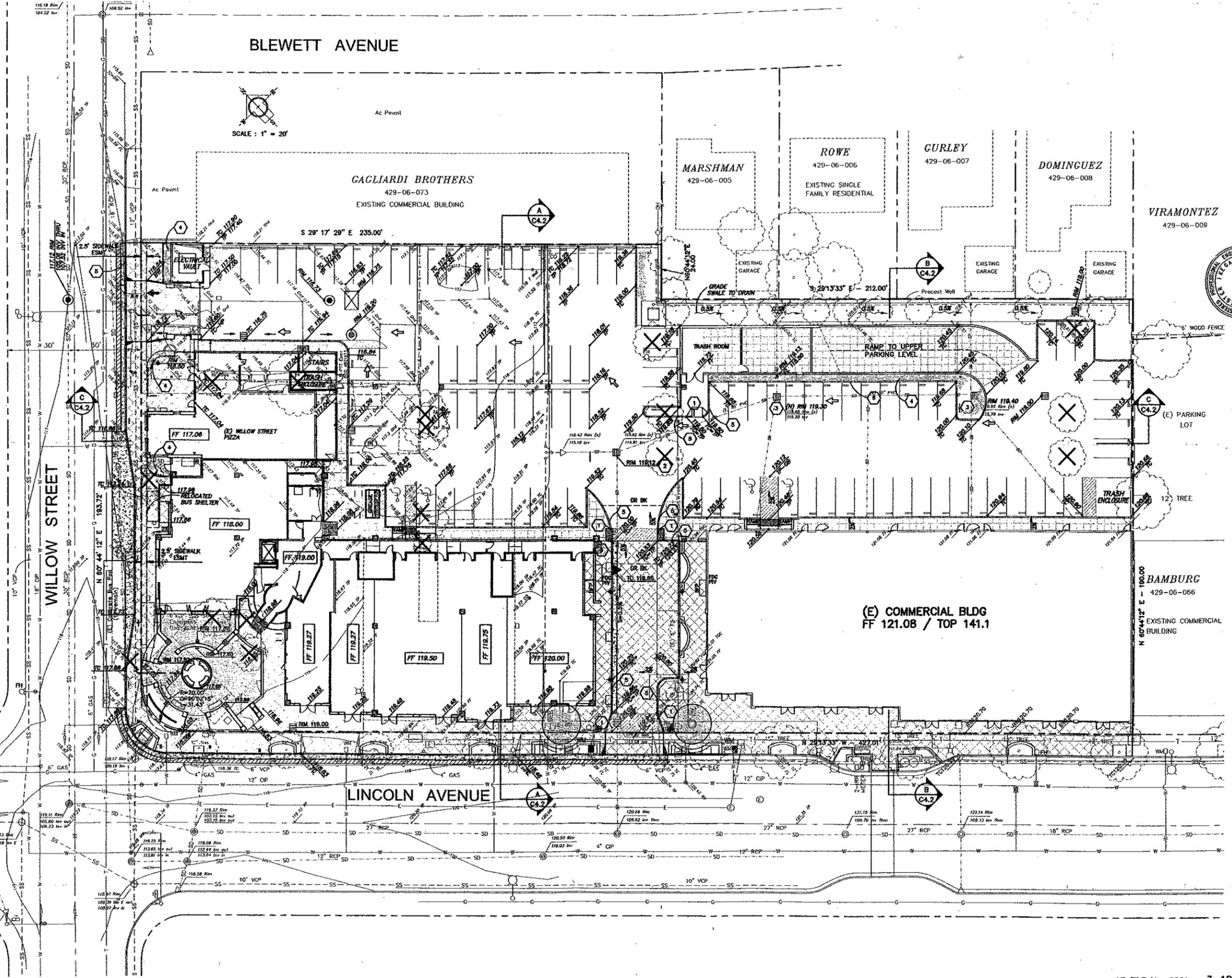
SHEET 1
OF 1 SHEETS
DRAWING NO.
0901

CONSTRUCTION NOTES

- 1 CURB FLUSH W/PAVEMENT
- 2 REPLACE (E) INLET
- 3 ADJUST (E) INLET TO GRADE
- 4 INSTALL FLOW THROUGH PLANTER
- 5 TAPER CURB HEIGHT 0" TO 6", OR AS SHOWN
- 6 INSTALL CURB OPENINGS PER DETAIL 7

7
C4.2

BLEWETT AVENUE



BENCHMARK
City of SJ #799
El 118.45

AE FILE No. 0901 3-18429



DATE	05/07/09
DRAWN BY	JO
CHECKED BY	LL
DATE	02/28/09
REVISION	3
DATE	02/17/09
REVISION	1
DATE	03/31/2010
REVISION	3
DATE	03/31/2010
REVISION	1

APPROVED BY: [Signature]

3170 WILLIAMS ROAD
SAN JOSE, CA 95128
CONSULTING ENGINEERS AND ARCHITECTS
LAND PLANNERS

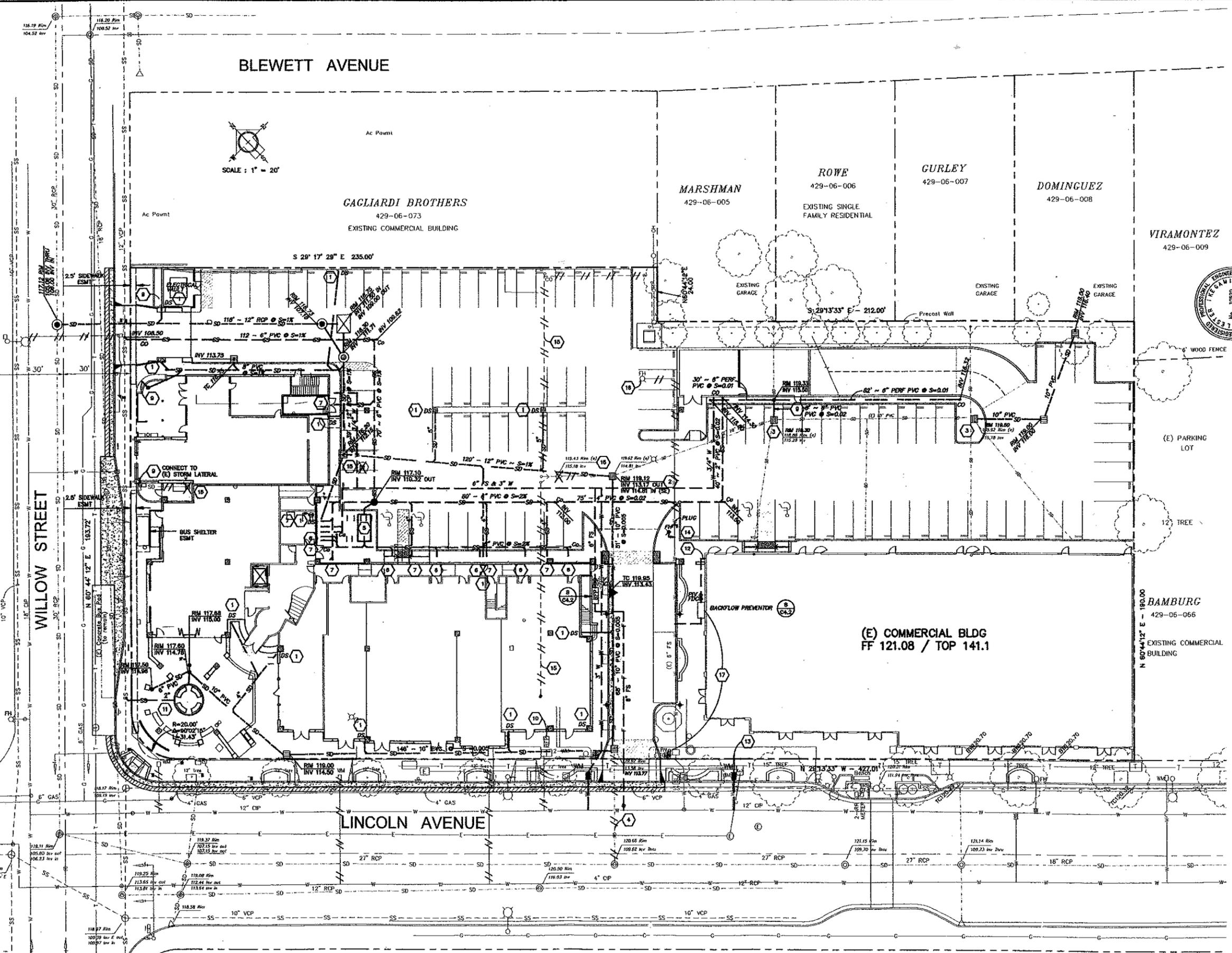
A DIVISION OF
ALLIED ENGINEERING COMPANY

PLANNED DEVELOPMENT (PD)
1104 LINCOLN AVENUE
SAN JOSE, CALIFORNIA

SHEET OF SHEETS
DRAWING NO. C4

CONSTRUCTION NOTES

- 1 CONNECT DS TO STORM SEWER W/ 4" PVC OR AS NOTED. SEE PLUMBING PLANS FOR CONNECTION DETAILS
- 2 REPLACE (E) INLET
- 3 ADJUST (E) INLET TO GRADE
- 4 PLUG & ABANDON (E) 12" RCP STORM LATERAL
- 5 1500 GALLON GREASE INTERCEPTOR JENSON PRECAST MODEL JP1500 EPE-G OR EQUAL
- 6 GREASE WASTE LINE SEE PLUMBING PLANS FOR CONTINUATION
- 7 4" SEWER LATERAL SEE PLUMBING PLANS FOR CONTINUATION
- 8 CONNECT DS TO DRAIN THROUGH CURB W/ 3" DIP
- 9 CONNECT FLOW-THROUGH PLANTER STORM DRAIN TO STORM LATERAL
- 10 (E) WATER METER & BFP TO BE REMOVED BY SJ WATER
- 11 INSTALL 2" PVC SEWER LATERAL TO FOUNTAIN
- 12 CONNECT TO (E) FIRE RISER
- 13 INSTALL (N) 3" WATER SERVICE, VERIFY LOCATION OF (E) SERVICE AT (E) COMMERCIAL BUILDING AND RECONNECT.
- 14 INSTALL STD CITY FIRE HYDRANT CONNECT TO (E) FIRE LINE
- 15 REMOVE INLET AND/OR LATERAL
- 16 REMOVE (E) FH AND FIRE LINE AND PLUG AT LOCATION SHOWN
- 17 REMOVE (E) DETECTOR CHECK, PIV AND FDC & INSTALL (N) ASSEMBLY IN RELOCATED POSITION.



BLEWETT AVENUE

WILLOW STREET

LINCOLN AVENUE

GAGLIARDI BROTHERS
429-06-073
EXISTING COMMERCIAL BUILDING

(E) COMMERCIAL BLDG
FF 121.08 / TOP 141.1

BAMBURG
429-06-066
EXISTING COMMERCIAL BUILDING



NOTE
FIRE LINE DESIGN IS CONCEPTUAL ONLY
FINAL DESIGN SHALL BE PROVIDED BY
FIRE DESIGN - BUILD CONTRACTOR

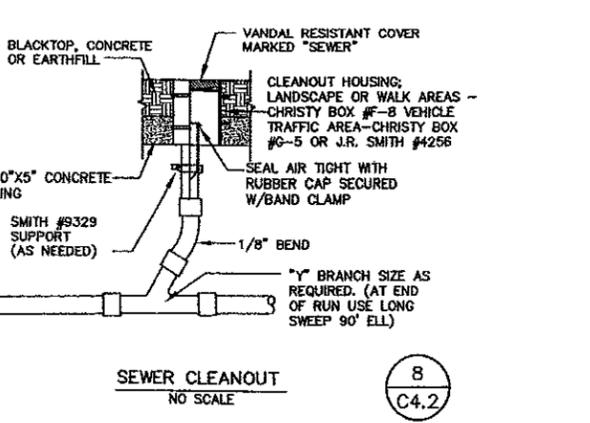
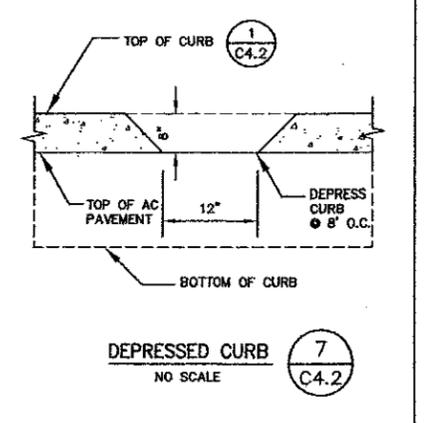
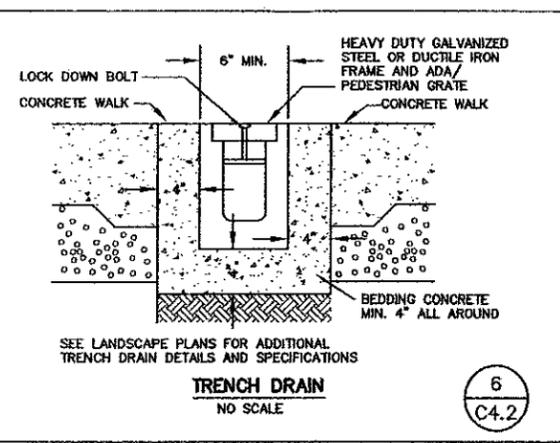
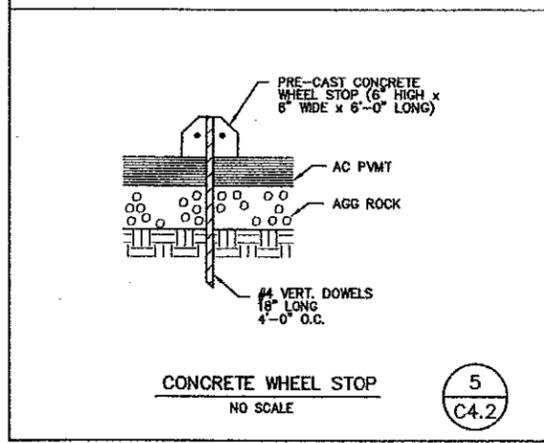
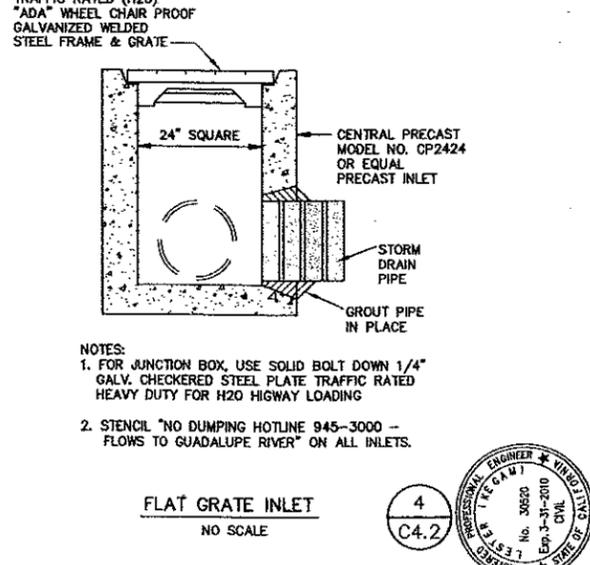
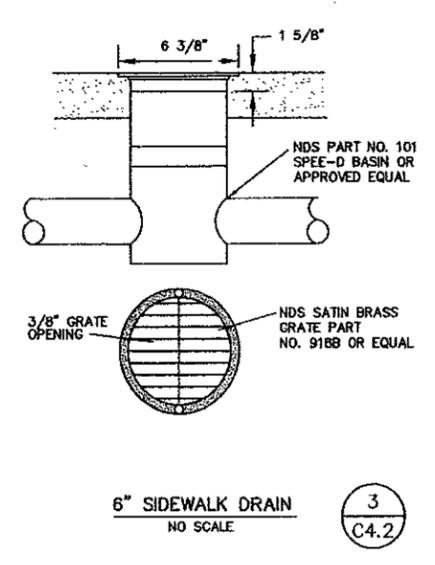
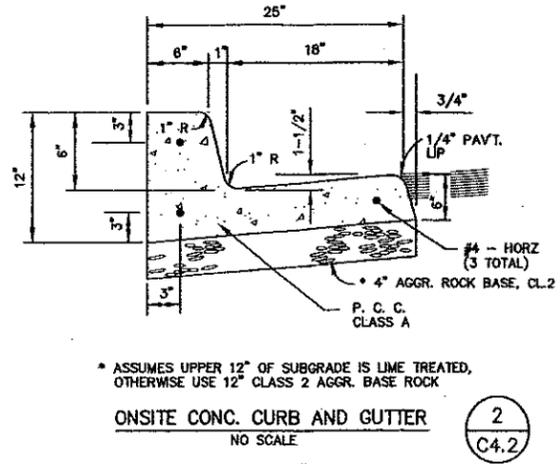
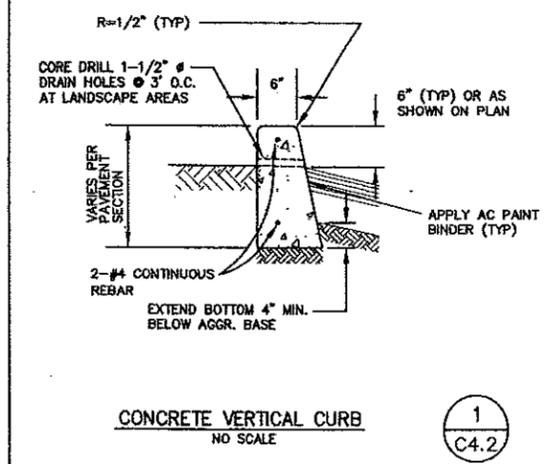
BENCHMARK
City of SJ #799
El 118.45



DATE	05/07/09
DRAWN BY	IC
CHECKED BY	LI
SCALE	1" = 20'
REVISION	
DATE	
APPROVED BY	
DATE	
LETTER	March 3-31-2010
EXPIRES	3-31-2010
REVISION #	3
REVISION #	1
PLANNED DEVELOPMENT (PD)	
UTILITY PLAN	
1104 LINCOLN AVENUE	
SAN JOSE, CALIFORNIA	
SHEET	
OF	
SHEETS	
DRAWING NO.	
C4.1	

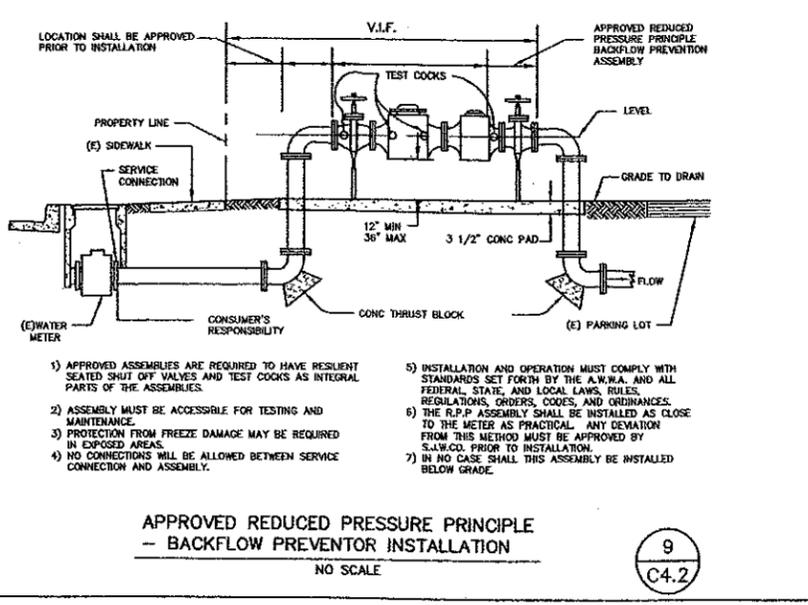
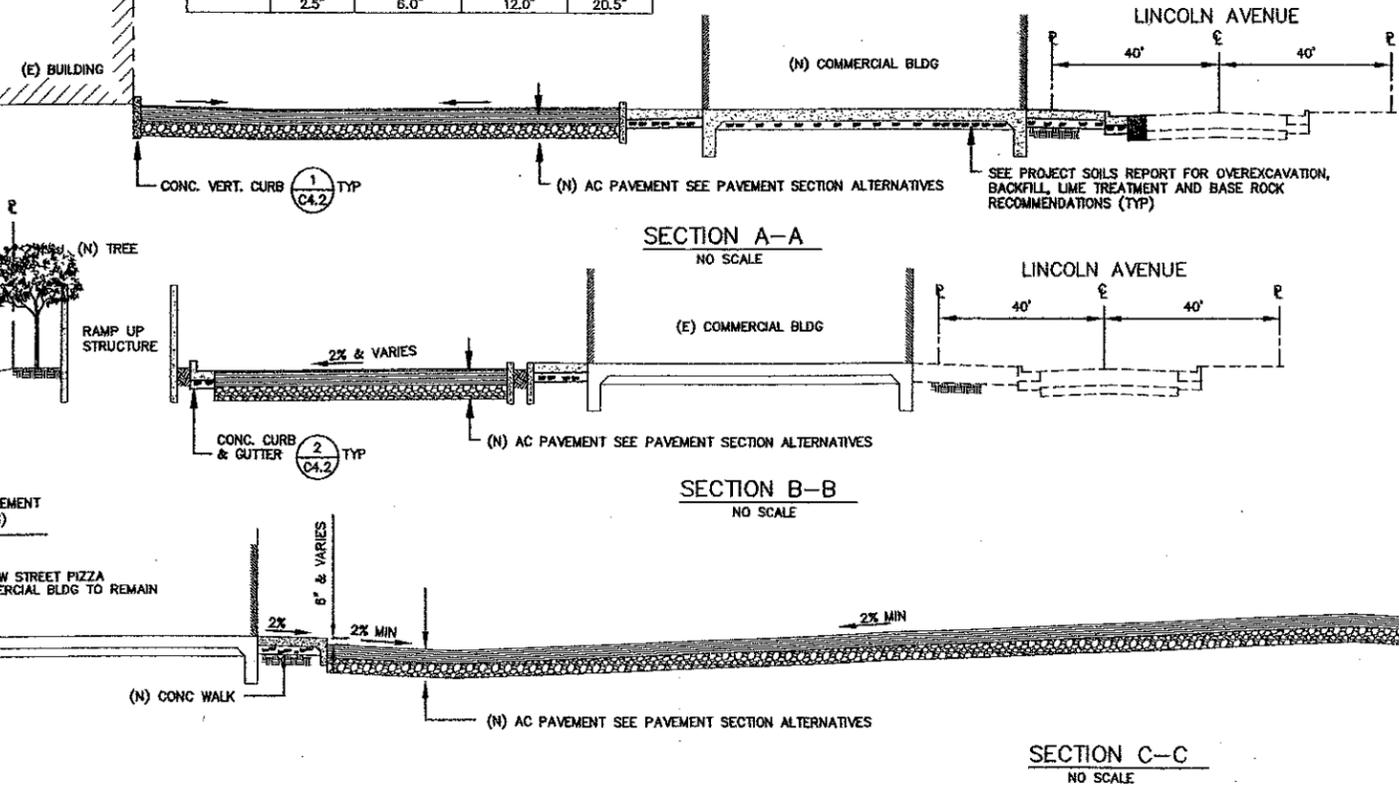
ON-SITE GRADING NOTES

- ROUGH PAD SHALL BE GRADED TO PLUS OR MINUS 0.10' OF DESIRED PAD ELEVATION AND WITHIN 0.05' OF LEVEL.
- ALL CONTRACTORS WILL BE RESPONSIBLE FOR THE VERIFICATIONS OF DEPTH AND LOCATIONS OF ALL EXISTING UTILITIES IN THE FIELD. UTILITY DATA MAY BE INCOMPLETE AND LOCATIONS SHOWN ON PLANS ARE APPROXIMATE AND FOR GENERAL INFORMATION ONLY.
- ALL GRADING AND EARTHWORK SHALL BE DONE IN ACCORDANCE WITH THE RECOMMENDATIONS AND UNDER THE SUPERVISION OF THE PROJECT SOILS ENGINEER.
- THE CONTRACTOR SHALL WORK WITH SOILS ENGINEER TO DETERMINE IF THE STRIPPING MATERIAL IS SUITABLE FOR LANDSCAPE USE. IF THE MATERIAL IS NOT SUITABLE OR IS IN EXCESS, THE CONTRACTOR IS RESPONSIBLE FOR DISPOSING OF ALL STRIPPINGS OFF-SITE. STRIPPINGS SUITABLE AS TOPSOIL SHALL BE STOCKPILED AT OWNER'S DIRECTION.
- ANY IMPORTED FILL MATERIAL SHALL BE APPROVED BY THE SOILS ENGINEER PRIOR TO BEING DELIVERED TO THE SITE.
- THE CONTRACTOR SHALL PROVIDE WATER FOR DUST CONTROL AND EARTH COMPACTION. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGE OR COMPLAINTS DUE TO DUST CAUSED BY HIS OPERATION. (SEE CITY OF SAN JOSE STD. GRADING PLAN NOTE NO. 5.)
- ALL CUT AND FILL SLOPES AT THE BOUNDARY LINES SHALL BE CONSTRUCTED IN SUCH A MANNER THAT ADJACENT FENCES WILL NOT BE DAMAGED. NO CONSTRUCTION WILL BE PERMITTED WITHIN 0.5 FEET OF FENCES WITHOUT PRIOR APPROVAL.
- EROSIONS CONTROL: ALL MOUNDING AND AREAS SUBJECT TO EROSION SHALL BE LANDSCAPED WITH EROSION CONTROL PLANTING AS PER LANDSCAPE ARCHITECT PLANS.
- THIS GRADING PLAN ILLUSTRATES THE PROPOSED GRADING FOR THE PROJECT. FINISH DRAINAGE SWALES AND DRAINAGE INLETS HAVE BEEN INDICATED HEREON AND MUST BE INCORPORATED INTO THE FINAL LANDSCAPE GRADING. IT SHALL BE THE RESPONSIBILITY OF THE OWNER AND HIS DESIGNATED CONTRACTOR(S) TO INSURE THAT THESE DRAINAGE SWALES AND INLETS ARE CONSTRUCTED AND ARE NOT OBSTRUCTED BY PLANTERS, SIDEWALKS, PAVING OR SIMILAR INSTALLATIONS. ANY PERMANENT SURFACING SUCH AS SIDEWALKS AND ASPHALT PAVING SHALL BE CONSTRUCTED WITH APPROPRIATE SWALES TO ACCOMMODATE DRAINAGE.
- PIPELINES WHICH ARE TO BE ABANDONED SHALL BE REMOVED BY CONTRACTOR IF THE OWNER'S SOILS ENGINEER SO REQUIRES. IF ALLOWED TO REMAIN, ALL OPEN ENDS, HOLES, OR OTHER OPENINGS IN THE PIPELINES SHALL BE PLUGGED. THE COST OF PLUGGING SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE GRADING ITEMS.
- THE CONTRACTOR SHALL SEAL ANY EXISTING WATER WELLS AND/OR MONITORING WELLS TO BE ABANDONED IN ACCORDANCE WITH THE LOCAL WATER DISTRICT AND CITY REQUIREMENTS. THE FINAL ELEVATION OF THE TOP OF WELL CASING MUST BE A MINIMUM OF 3 FEET BELOW THE ADJACENT GRADE PRIOR TO ANY GRADING OPERATION. CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING WELLS AND NOTIFY OWNER OF LOCATION PRIOR TO START OF GRADING.
- ALL CONTRACTORS ARE TO OBTAIN THEIR RESPECTIVE PERMITS FROM THE AGENCIES HAVING JURISDICTION OVER THEIR WORK AND SHALL ADVISE THE AGENCY 48 HOURS PRIOR TO STARTING WORK.
- APPROVAL OF THIS PLAN APPLIES ONLY TO THE EXCAVATION, PLACEMENT AND COMPACTION OF NATURAL EARTH MATERIALS, AND TO THE DRAINAGE AND DRAINAGE INLET STRUCTURES. CONTRACTORS SHALL CONSTRUCT ALL IMPROVEMENTS TO THE RECOMMENDATION AND SPECIFICATIONS OF THE SOILS ENGINEER.
- THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE AMOUNT OF ENGINEERED FILL IMPORT MATERIAL THAT IS TO BE REQUIRED, AND IS ALSO RESPONSIBLE FOR DETERMINING THE AMOUNT OF EXCESS EXPORT MATERIAL THAT MAY BE REQUIRED TO BE REMOVED FROM THE SITE.
- ALL EXISTING CONCRETE SIDEWALK AND CURB AND GUTTER BROKEN OR DAMAGED DURING CONSTRUCTION SHALL BE REMOVED AND REPLACED TO THE SATISFACTION OF THE CITY ENGINEER.
- ALL CONTRACTORS WILL BE RESPONSIBLE FOR THE VERIFICATION OF LOCATIONS OF ALL EXISTING UTILITIES IN THE FIELD. ALL CONTRACTORS SHALL CALL UNDERGROUND SERVICE ALERT (U.S.A.) (TEL. 1-800-842-2444) 48 HOURS BEFORE DIGGING. EXCAVATION FOR UNDERGROUND FACILITIES SHALL NOT BE PERMITTED PRIOR TO CONTACTING UNDERGROUND SERVICE ALERT. UNDER NO CIRCUMSTANCES WILL EXCAVATION COMMENCE BEFORE BEING ISSUED AN IDENTIFICATION NUMBER FROM U.S.A. (SECTION 4210.1 OF THE GOVERNMENT CODE).
- DEPTH OF OVEREXCAVATION WILL BE DETERMINED IN THE FIELD BY SOILS ENGINEER.
- BENCHMARK: BENCHMARK NO. 799 THE LETTER "I" IN THE WORD IRON, TOP OF CATCH BASIN ON NORTHWESTERLY RETURN OF WILLOW STREET AND LINCOLN AVENUE. ELEVATION = 118.45 (NGVD 29)
- LAYOUT NOTE: VERIFY EXACT LOCATION AND DIMENSIONS OF PROPOSED DRIVEWAY, PARKING AREAS, WALKS, RAMPS AND TRASH AREAS W/ARCHITECT AND/OR OWNER PRIOR TO START OF WORK.

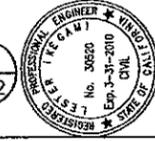


PAVEMENT SECTION ALTERNATIVES

	AC	CLASS 2 AB	LIME TREATED SUBGRADE	TOTAL THICKNESS
AUTO PARKING	3.0"	8.0"	-	12.0"
PARKING	2.0"	4.0"	12.0"	18.0"
DRIVEWAY	4.0"	11.0"	-	15.0"
	2.5"	6.0"	12.0"	20.5"



- APPROVED ASSEMBLIES ARE REQUIRED TO HAVE RESILIENT SEATED SHUT OFF VALVES AND TEST COCKS AS INTEGRAL PARTS OF THE ASSEMBLIES.
- ASSEMBLY MUST BE ACCESSIBLE FOR TESTING AND MAINTENANCE.
- PROTECTION FROM FREEZE DAMAGE MAY BE REQUIRED IN EXPOSED AREAS.
- NO CONNECTIONS WILL BE ALLOWED BETWEEN SERVICE CONNECTION AND ASSEMBLY.
- INSTALLATION AND OPERATION MUST COMPLY WITH STANDARDS SET FORTH BY THE A.W.W.A. AND ALL FEDERAL, STATE, AND LOCAL LAWS, RULES, REGULATIONS, ORDERS, CODES, AND ORDINANCES.
- THE R.P.P. ASSEMBLY SHALL BE INSTALLED AS CLOSE TO THE METER AS PRACTICAL. ANY DEVIATION FROM THIS METHOD MUST BE APPROVED BY S.J.W.C.D. PRIOR TO INSTALLATION.
- IN NO CASE SHALL THIS ASSEMBLY BE INSTALLED BELOW GRADE.



DATE: 05/07/09
DRAWN BY: LD
CHECKED BY: LI
SCALE: AS SHOWN

REVISION
DATE
REVISION # 1
DATE
REVISION # 2
DATE
REVISION # 3
DATE
REVISION # 4

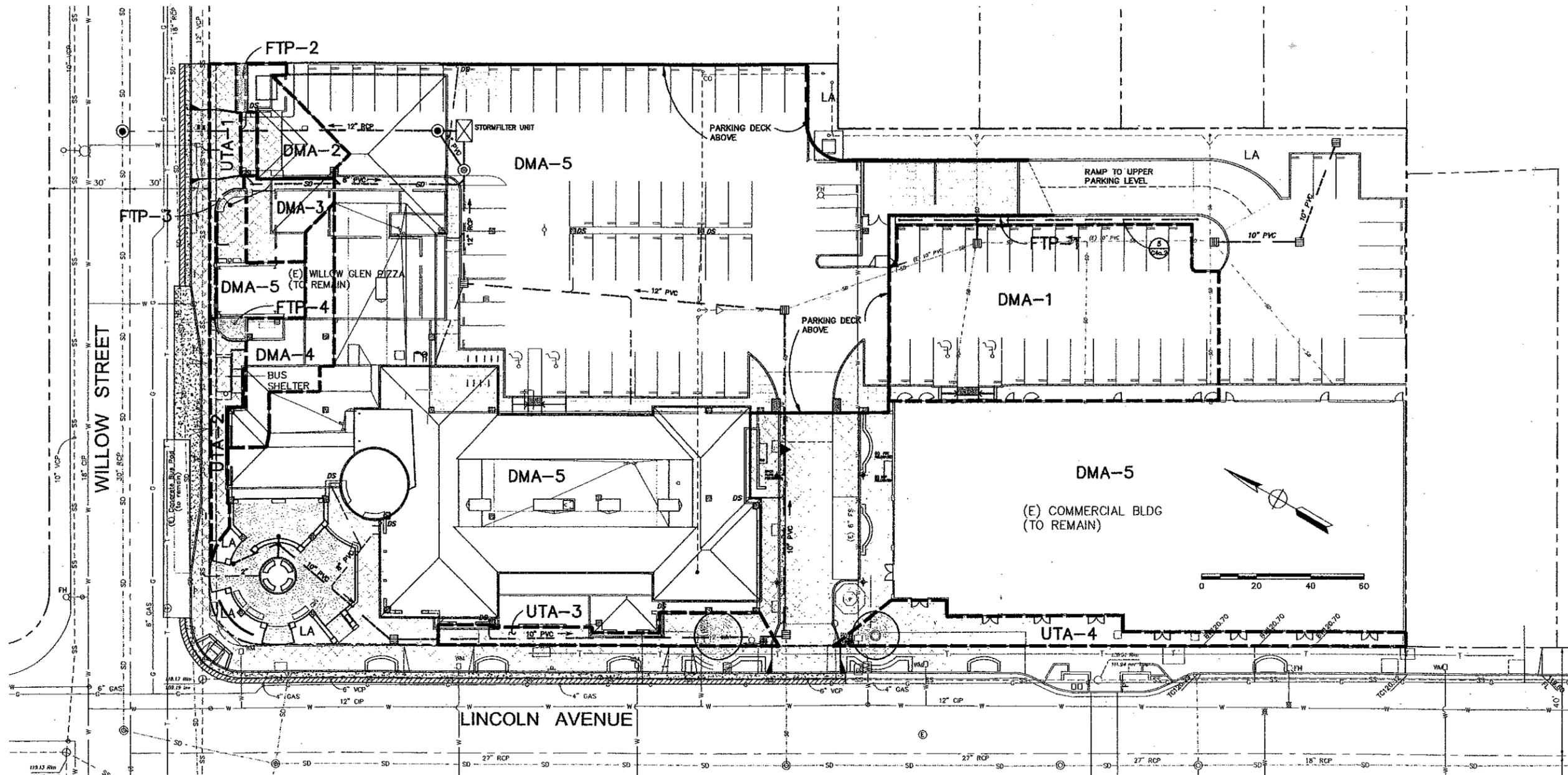
APPROVED BY:
DATE
Lester Ikegami, RCE 30520
Expires 3-31-2010

CONSULTING ENGINEER
3175 WILLIAMS BLVD
SAN JOSE, CA 95131
(408) 241-1869

PLANNED DEVELOPMENT (PD)
NOTES, DETAILS & SECTIONS
1104 LINCOLN AVENUE
SAN JOSE, CALIFORNIA

SHEET
OF
SHEETS
DRAWING NO.
C4.2

AE FILE No. 0901 3-18429



LEGEND

- LA LANDSCAPE AREA
- DMA DRAINAGE MANAGEMENT AREA
- FTP FLOW-THROUGH PLANTER
- UTA UNTREATED AREA
- ⊕ STORMGATE MH $\frac{1}{C4a.2}$ & $\frac{2}{C4a.2}$
- ⊗ STORMFILTER VAULT (8'x6') $\frac{4}{C4a.2}$
- ▨ FLOW THROUGH PLANTER $\frac{5}{C4a.2}$

GENERAL PROJECT INFORMATION

PROJECT NAME	1104 LINCOLN AVENUE	EXISTING BUILDINGS	FOUR (TO BE REMOVED) TWO (TO REMAIN)
RECORD OWNER & DEVELOPER	ALEX BYER / PAJA INVESTMENTS 66 POINERRO AVENUE SAN FRANCISCO CA 94103 TEL (415) 626-2937 FAX (415)	RECEIVING WATER BODY	CITY STORM TO GUADALUPE RIVER (4000' ± EAST OF SITE)
CIVIL ENGINEER	LESTER IKEGAMI R.C.E. 30,520 EXPIRES MARCH 31, 2010 ALLIED ENGINEERING COMPANY 3170 WILLIAMS ROAD SAN JOSE, CA 95117 TEL (408) 241-1960 FAX (408) 241-3047	WATER SUPPLY	SAN JOSE WATER CO.
ASSESSOR'S PCL NO.	429-06-001, 002, 067, 069 & 429-06-070	PROVISIONAL RECYCLED WATER	NOT AVAILABLE
PROPOSED USE	COMMERCIAL BUILDING	SOLID WASTE HAULER	GREENTEAM OF SAN JOSE
EXISTING USE	COMMERCIAL BUILDING	NATURAL HYDRAULIC FEATURES	NONE
GENERAL PLAN/DESIGNATION	GC (GENERAL COMMERCIAL)	SOIL TYPES	SILTY CLAY
		DEPTH OF GROUND WATER	NOT ENCOUNTERED PER SOILS REPORT (HISTORICALLY 40 TO 50 FEET BELOW GROUND LEVEL)
		FLOOD ZONE	ZONE D, FIRM PANEL NO. 060349 0031 D DATED AUGUST 2, 1982
		TOTAL NET SITE AREA	2.077 ± ACRES



DATE 05/07/09
DRAWN BY: IO
CHECKED BY: LI
SCALE: 1" = 20'

APPROVED BY:
DATE 12/29/09 REVISION # 3
DATE 09/17/09 REVISION # 1

REGISTERED PROFESSIONAL ENGINEER
LESTER I. IKEGAMI
No. 30520
Exp. 3-31-2010
CIVIL
ALLIED ENGINEERING COMPANY

3170 WILLIAMS ROAD
SAN JOSE, CA 95117
(408) 241-1960
CONSULTING CIVIL ENGINEERS AND SURVEYORS AND FOUNDERS

APPROVED BY:
DATE 12/29/09 REVISION # 3
DATE 09/17/09 REVISION # 1

ALLIED ENGINEERING COMPANY
3170 WILLIAMS ROAD
SAN JOSE, CA 95117
(408) 241-1960
CONSULTING CIVIL ENGINEERS AND SURVEYORS AND FOUNDERS

PLANNED DEVELOPMENT (PD)
STORMWATER CONTROL PLAN
1104 LINCOLN AVENUE
SAN JOSE, CALIFORNIA

SHEET OF SHEETS
DRAWING NO. C4a

BEST PRACTICE MEASURES

SITE CONSTRAINTS

- The existing soils as identified by the project Geotechnical Engineer has been identified as firm to locally stiff clay making deep infiltration by landscaped-based or any other type of BMP's not feasible.
- There are several areas of onsite paved areas located between the front of the buildings and the city sidewalk that will be surface draining to the street untreated. The project "Tree Credit" will be used to account for the three Untreated Areas (UTA) in front of the new building (UTA-1, UTA-2 and UTA-3).
- The paved area in front of the existing commercial building (UTA-4) is being replaced to match the rest of the project aesthetically. There is no opportunity to treat this water and is unaccounted for in this plan.

SOURCE CONTROL MEASURES

- Keep outside area neat and clean
- Provide covered trash receptacles for customers and employees
- Maintain cleaning equipment leak/drip free
- Instruct employees regularly of good housekeeping practices
- Post signage on good housekeeping and to avoid washing pollutants from discharging to municipal sanitary sewer system (SSS)
- Inspect & clean storm drain inlets before Oct. 1 each year
- Maintain the sanitary process wastewater sewer separate from contaminating the Storm Drainage System (SDS)
- Instruct employees on spill control and prevention
- Provide and make available spill cleanup kit
- Routinely service and clean oil, grease and sediment interceptors for SSS
- Properly store and dispose of detergents, cleaning compounds and solvents properly to avoid contamination of SDS
- Labeling of storm water inlets
Storm water inlets will be labeled with the logo "No Dumping Hot line 945-3000 / Flows to Guadalupe River". This educational measure is intended to prevent unlawful dumping of waste materials, such as motor oil, into storm drains

STORMWATER BMP'S

FLOW-THROUGH PLANTERS (FTP)

- Roof drainage from DMA-2, and DMA-3 will be directed to 2 Raised Flow Through Planters. These BMP's will be treatment-only facilities. See Table 2 for the BMP area calculations and Details 5/4.1b for typical planter details. Once treated, the flow from FTP-2 & FTP-3 will tie to the onsite storm sewer system upstream of the StormFilter unit and be treated by that BMP also. The flow from DMA-4 and FTP-4 will tie directly into the City storm sewer system downstream of the StormFilter unit.
- The parking lot drainage from the Drainage Management Area, DMA-1, will be directed to drain to an at grade Flow-Through Planter along the northeast side of the parking lot. This BMP will be a treatment-only facility. See Table 2 for BMP area calculations and Detail.5/4a.2 for typical planter details. Once treated, the flow from DMA-1 will tie to the onsite storm sewer system upstream of the StormFilter unit and will be treated by that BMP also.
- Maintenance :
 - Maintain vegetation and irrigation systems. Inspect periodically and after storms to ensure structural integrity and that planter has not clogged
 - The use of pesticides and quick release of synthetic fertilizers shall be minimized, and the principles of integrated pest management followed check with local jurisdiction for any local police regarding the use of pesticides and fertilizers.

STORMFILTER VAULT - MEDIA FILTRATION SYSTEM WITH STORMGATE HIGH FLOW BYPASS

The StormFilter consists of an 8x6 precast concrete vault with 10 cartridges to treat the stormwater. The StormGate is a high flow bypass used to address the high flows in excess of the design flows that may occur. The majority of runoff will enter the StormGate manhole by an inlet pipe discharging directly into the manhole. A small amount of surface flow will enter the manhole by an open grate cover. The StormGate uses a field adjustable weir to direct the polluted low flows to the StormFilter vault, while allowing high flows to bypass the facility.

The StormFilter system shall be maintained in accordance with the manufacturer's recommendations. The owner shall enter into a maintenance agreement with a professional service company to carry out maintenance activities including:

Inspection:

- Minimum once per year
- After major storms
- In the event of a chemical spill
- Per regulation requirements of governing agency

Maintenance:

- Replace cartridges and remove sediment on an "as-needed" basis per manufacturer's recommendations
- Material Directions Disposal: Water and sediment shall be disposed of in accordance with all applicable waste disposal regulations.
- Mosquito abatement: Abatement measures for standby water shall be performed in accordance with County of Santa Clara regulations

STORM WATER CALCULATIONS FOR STORMFILTER UNIT

I. Contributing Areas :

$$\begin{aligned} \text{Total Area} &= \text{DMA-1} + \text{DMA-2} + \text{DMA-3} + \text{DMA-5} \\ &= 7801 + 925 + 814 + 75,533 \\ &= 85,073 = 1.95 \text{ Acres} \\ &43,560 \end{aligned}$$

II. Required sizing for flow based Media Filtration System BMP design

Runoff coefficient: C=0.80 (Commercial)
Rainfall intensity: I= 0.2 Inches/Hour
Total Area: A= 1.95 Acres

$$\begin{aligned} Q &= C * I * A = (0.80)(0.2)(1.95) = 0.31 \text{ Cubic Ft / Sec} \\ Q &= 0.31 \text{ cfs} \times 449 \text{ gpm/cfs} = 139 \text{ gpm} \\ \text{Number of cartridges} &= 139 \text{ gpm} / 15 \text{ gpm/cart} = 10 \text{ cartridges} \end{aligned}$$

III. For "10 Year" Storm (Peak Flow)
Annual Rainfall= 15 Inches
I (10)= 2 Inches/ Hr

$$\begin{aligned} Q(10) &= (0.80) * (2.0) * (1.95) = 3.12 \text{ Cubic Ft/ Sec} \\ Q_{full} \text{ for } 12" \text{ RCP @ } S=1\% &= 3.3 \text{ cfs, therefore is adequate} \end{aligned}$$

SITE:	EXISTING CONDITION (SQ FT)	%	PROPOSED CONDITION (SQ FT)	%	DIFFERENCE (SQ FT)	%
2.077 Acres/90,476 S.F.						
Building Footprints	26,159	29	33,347	37	7,188	8%
Parking (Paved Areas)	42,227	47	37,800	42	-4,627	-5%
Sidewalks, Plaza, etc.	11,956	13	15,248	16	3,292	4%
Landscaping	10,134	11	4,281	5	-5,853	-6%
Total	90,476	100	90,476	100	0	
Impervious Surfaces	80,342	89	86,195	95	5,853	
Pervious Surfaces	10,134	11	4,281	5	-5,853	
Total	90,476	100	90,476	100		

AREA ID	IMPERVIOUS AREA (SQ FT)	PERVIOUS AREA (SQ FT)	TOTAL AREA (SQ FT)	MIN SURFACE AREAS (SIZING FACTOR =0.4) (SQ FT)	SURFACE AREAS AS DESIGNED (SQ FT)
DMA-1	7,801	0	7,801	312	316
DMA-2	925	0	925	37	54
DMA-3	814	0	814	33	33
DMA-4	1,275	0	1,275	51	78
TOTAL	10,815	0	10,815	-	-

AREA ID	IMPERVIOUS AREA (SQ FT)	PERVIOUS AREA (SQ FT)	TOTAL AREA (SQ FT)
UTA-1	611	0	611
UTA-2	708	0	708
UTA-3	1,017	0	1,017
UTA-4	1,792	0	1,792
SUB TOTAL	2,336	0	2,336
TOTAL	4,128	0	4,128

TREE CREDIT CALCULATION

$$\begin{aligned} 21 \text{ DECIDUOUS TREES} \times 100 \text{ SF/TREE} &= 2,100 \text{ SF} \\ 6 \text{ EVERGREEN TREES} \times 200 \text{ SF/TREE} &= 1,200 \text{ SF} \\ \text{TOTAL CREDIT AREA} &= 3,300 \text{ SF} \end{aligned}$$

$$\text{TOTAL AREA FROM UTA-1, 2 \& 3} = 2,336 \text{ SF}$$

OK



DATE: 05/07/09
DRAWN BY: JI
CHECKED BY: JI
SCALE: AS SHOWN

REVISION
DATE: 12/29/09
REVISION # 3
DATE: 09/17/09
REVISION # 1

APPROVED BY:
Lester Negroni, RCE 30520
Expires 3-31-2010

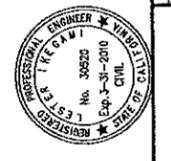
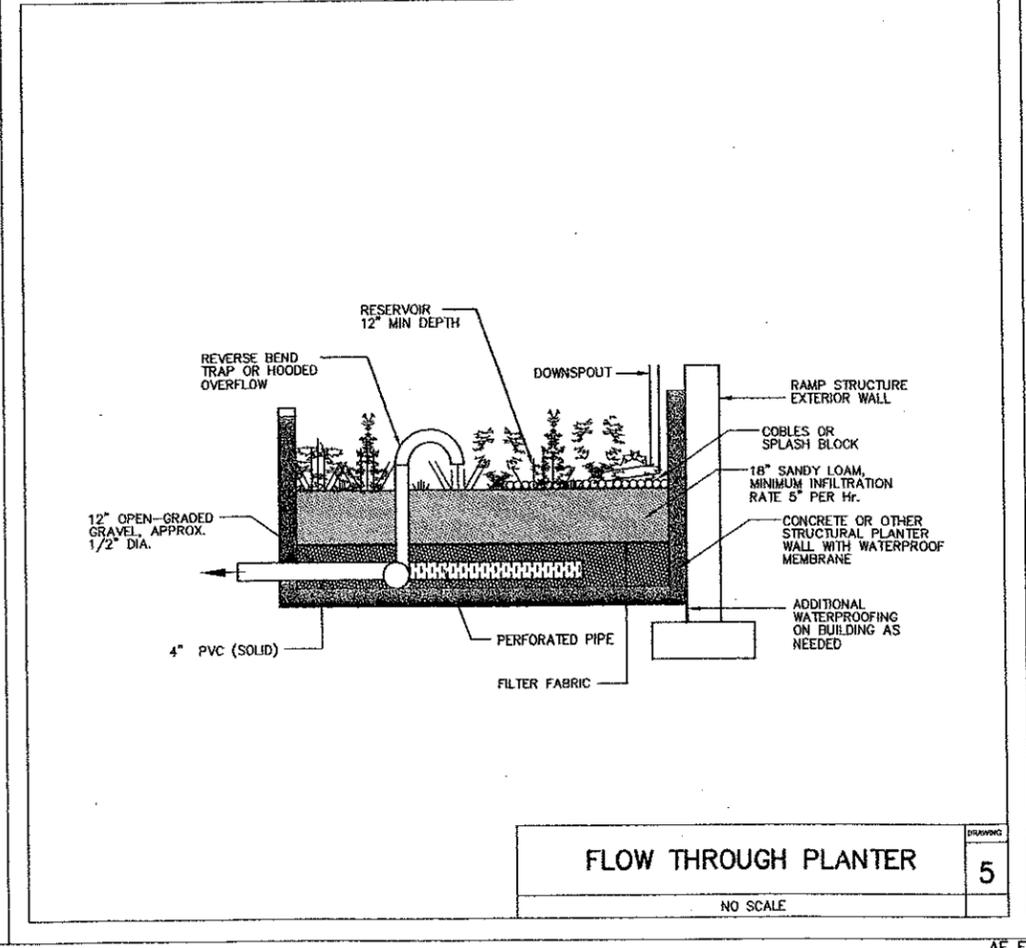
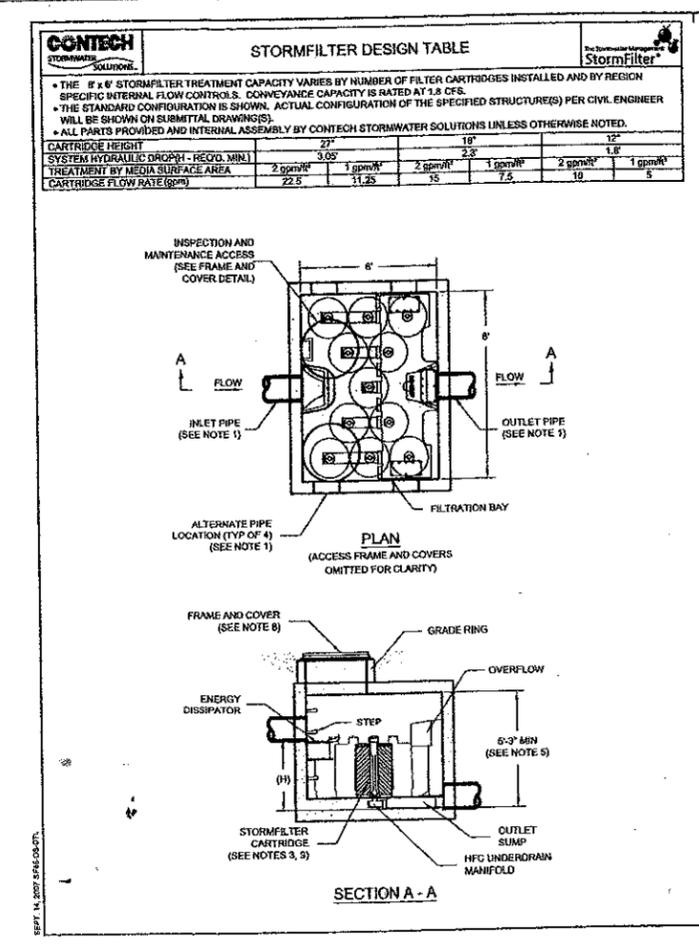
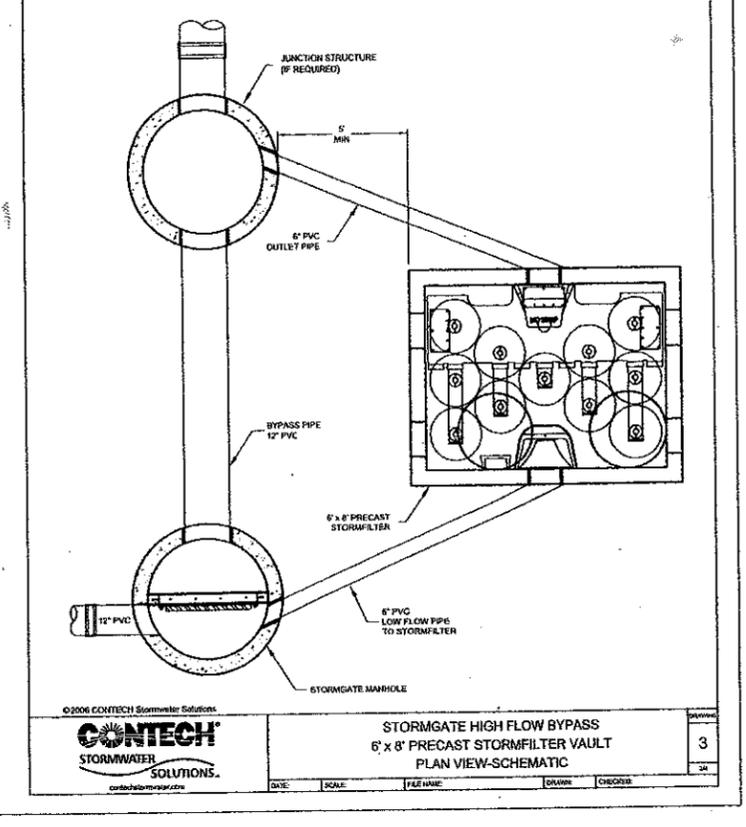
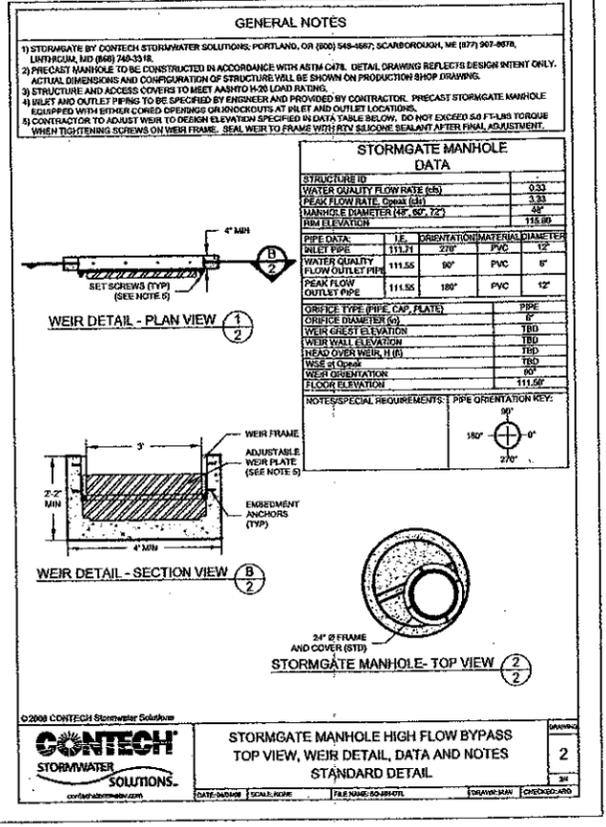
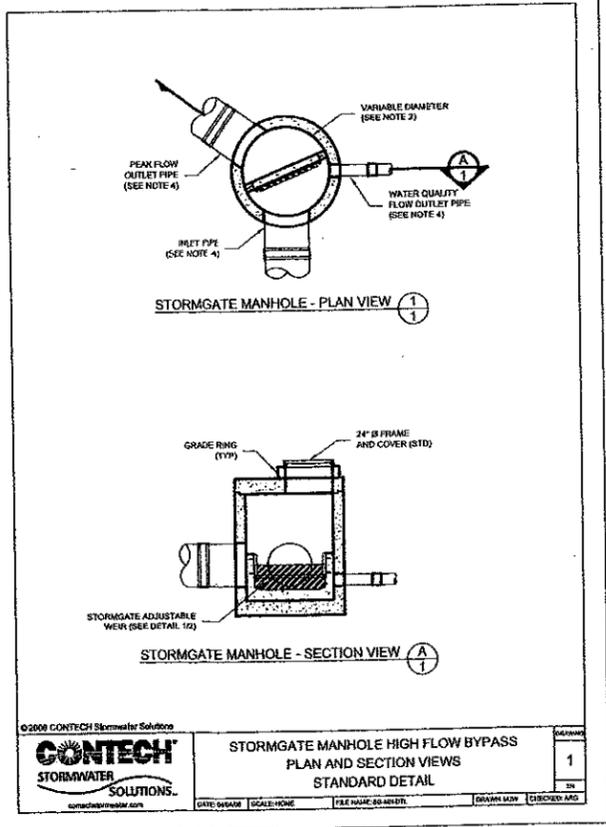
3170 WILLIAMS ROAD
SAN JOSE, CA 95117
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ALLIED
ENGINEERING
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A DIVISION OF
CONSULTING
CIVIL ENGINEERS
AND PLANNERS

PLANNED DEVELOPMENT (PD)
STORMWATER CONTROL NOTES
1104 LINCOLN AVENUE
SAN JOSE, CALIFORNIA

SHEET _____
OF _____
SHEETS

DRAWING NO.
C4a.1



DATE: 05/07/09
 DRAWN BY: IO
 CHECKED BY: LI
 SCALE: AS SHOWN

APPROVED BY:
 DATE: 12/29/08
 REVISION # 3
 Expires 3-31-2010

3170 WILLIAMS ROAD
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CONSULTING
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PLANNED DEVELOPMENT (PD)
 STORMWATER CONTROL DETAILS
 1104 LINCOLN AVENUE
 SAN JOSE, CALIFORNIA

SHEET _____ OF _____ SHEETS
 DRAWING NO. C4a.2